

Public Works Standard Drawings Subcommittee Meeting

Monday, April 13, 2020

WebEx

Welcome and Introductions

Meeting Summary

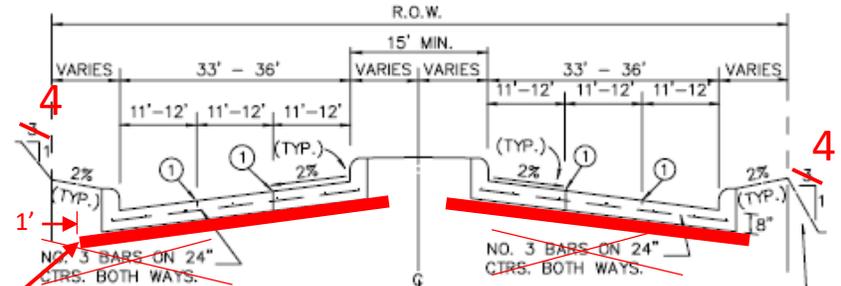
Division 2000: Pavement Systems

DIVISION 2000 PAVEMENT SYSTEMS**TABLE OF CONTENTS**

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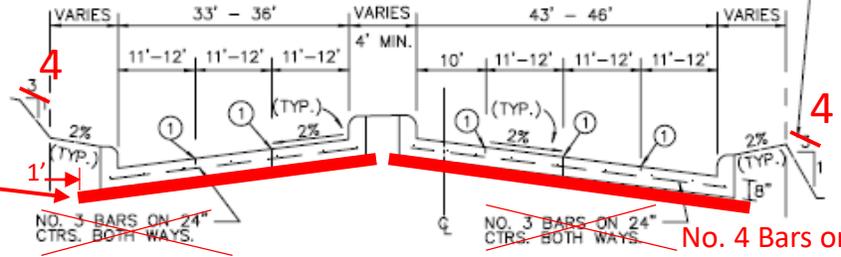
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8" minimum stabilized subgrade per section 301 and as approved or specified by owner



REGULAR SECTION
N.T.S.

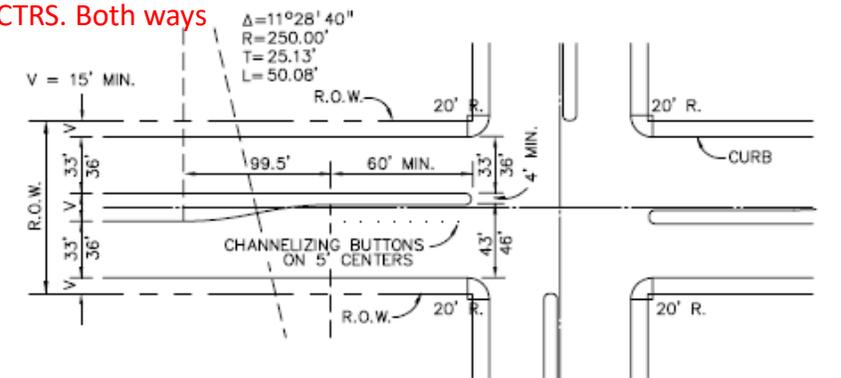
No. 4 Bars on 18" CTRS. Both ways



LEFT TURN SECTION
N.T.S.

No. 4 Bars on 18" CTRS. Both ways

No. 4 Bars on 18" CTRS. Both ways



NOTES:

1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 8" - CLASS "C" OR "PC", OR AS SPECIFIED BY OWNER.
2. MIN. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY OWNER.
3. ~~ALTERNATE REINFORCEMENT SHALL BE #4 BARS ON 30" CENTERS BOTH WAYS.~~

PLAN
N.T.S.

① SAWED LONGITUDINAL CONTRACTION JOINT OR CONSTRUCTION JOINT.

REINFORCED CONCRETE PAVEMENT
SIX-LANE DIVIDED THOROUGHFARE



STANDARD SPECIFICATION REFERENCE
303
DATE: OCT. '04
STANDARD DRAWING NO. 2010

Replace Plan with a more general layout to include crosswalks, ADA ramps, striping, and possibly additional lane width for bicycle lanes per TxDOT

8" minimum stabilized subgrade per section 301 and as approved or specified by owner

No. 4 Bars on 18" CTRS. Both ways

NOTES:

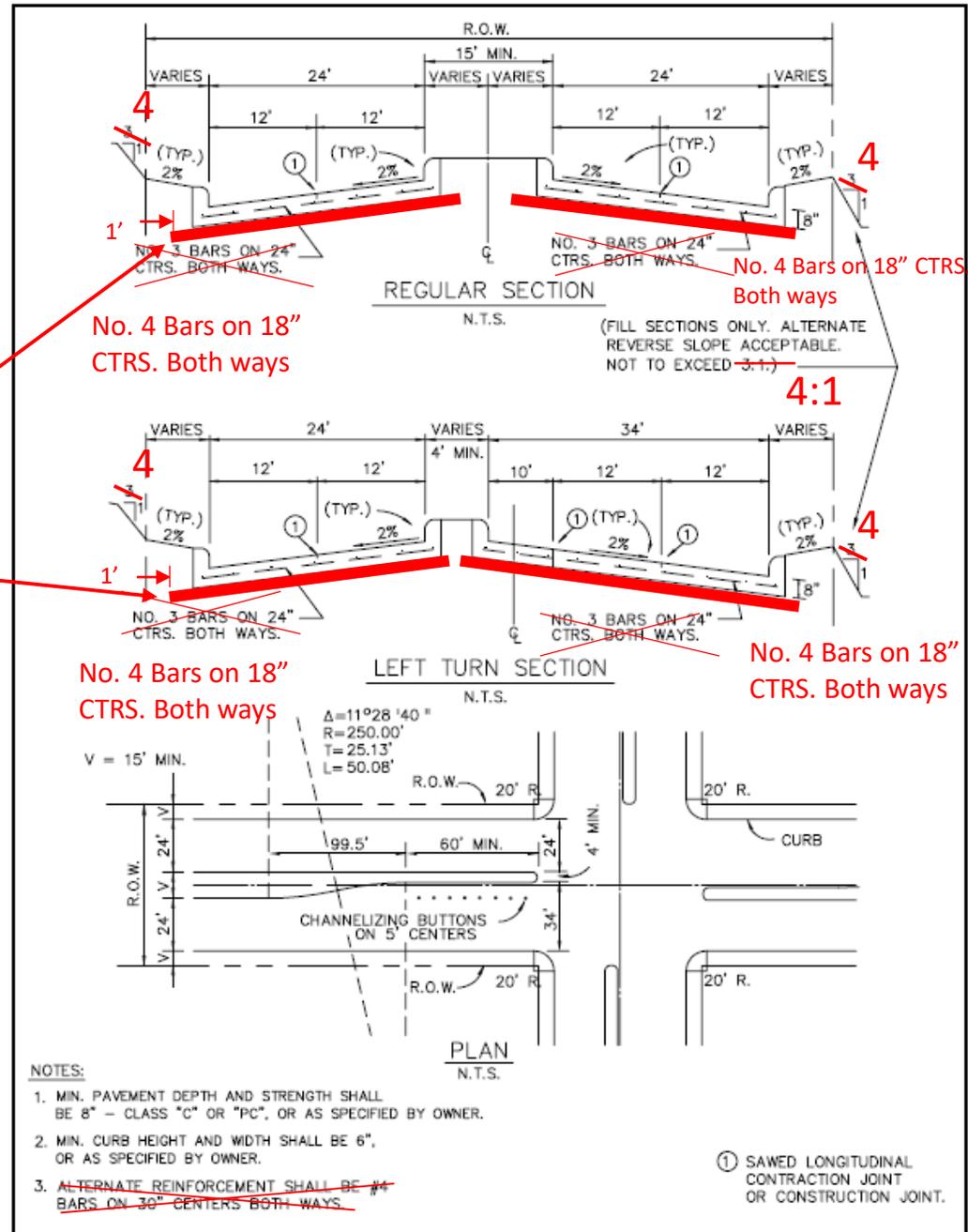
1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 8" - CLASS "C" OR "PC", OR AS SPECIFIED BY OWNER.
2. MIN. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY OWNER.
3. ~~ALTERNATE REINFORCEMENT SHALL BE #4 BARS ON 30" CENTERS BOTH WAYS.~~

① SAWED LONGITUDINAL CONTRACTION JOINT OR CONSTRUCTION JOINT.

REINFORCED CONCRETE PAVEMENT
FOUR-LANE DIVIDED THOROUGHFARE



STANDARD SPECIFICATION REFERENCE
303
DATE
OCT. '04
STANDARD DRAWING NO.
2020

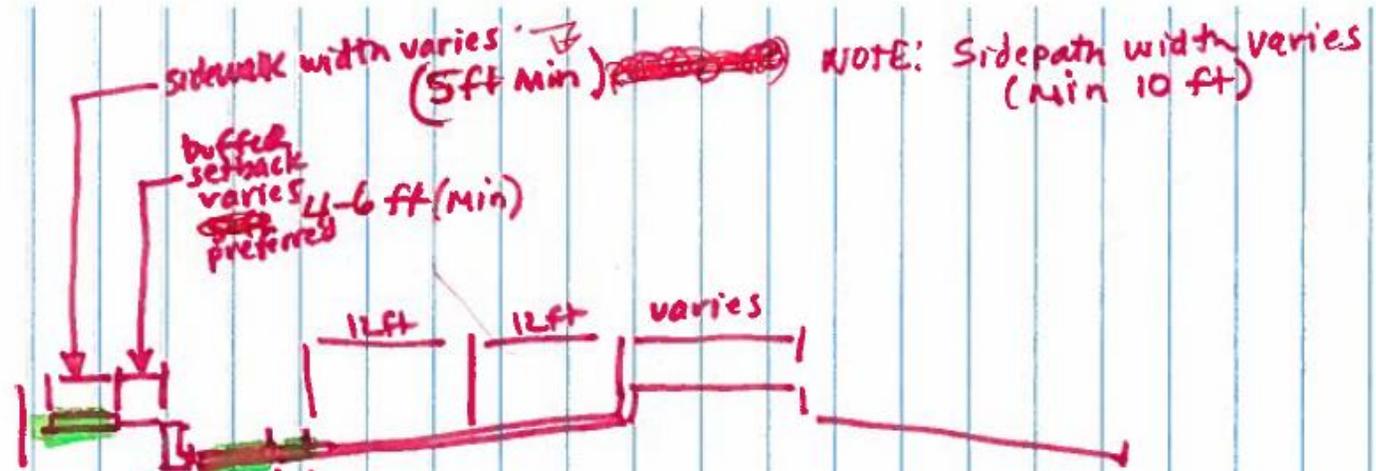


Replace Plan with a more general layout to include crosswalks, ADA ramps, striping, and possibly additional lane width for bicycle lanes per TxDOT

3. Alternative subgrade, thickness, and steel may be utilized with more detailed study and analysis and as approved by owner

4. If lime stabilized subgrade is utilized a minimum of 40 lbs/sy is required

5. See detail 2170 for sidewalks



NOTE: Sidepath width varies (Min 10 ft)

Buffer
varies

ref ch.2 (Forms of Separation)
FHWA Sep. Bike Lane
Planning and Design
Guide, May 2015

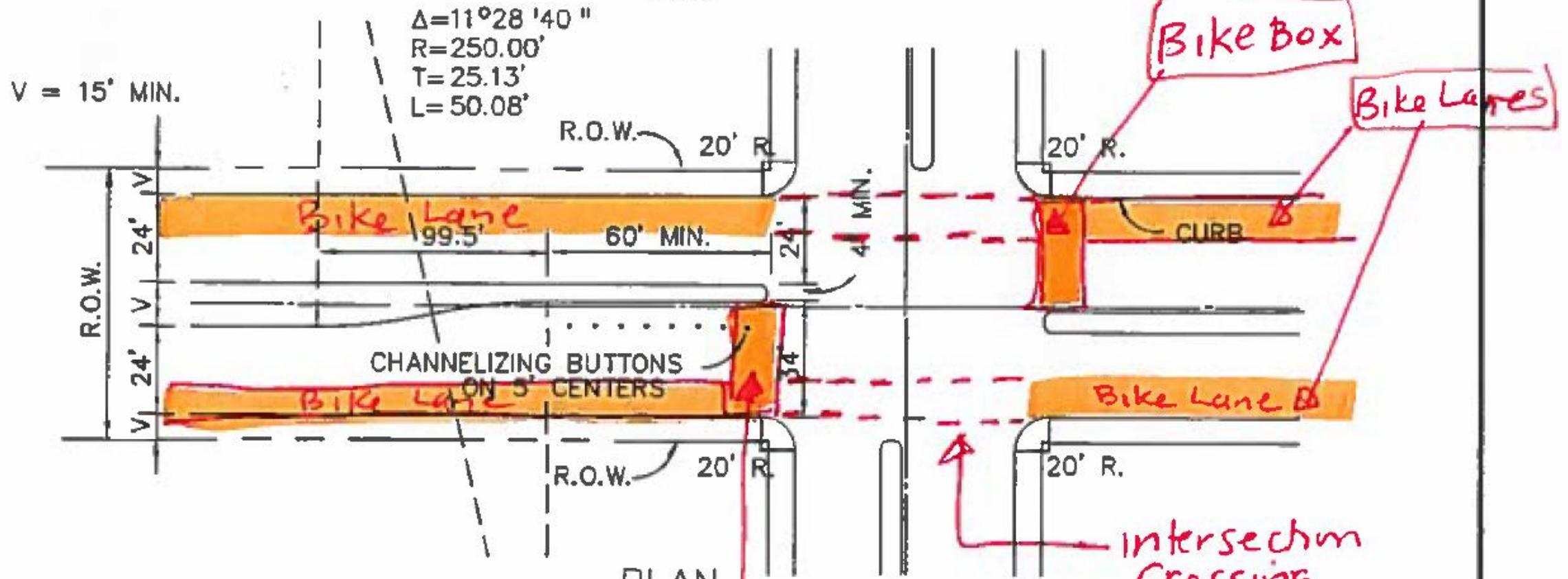
Bike Lane

5-7 ft
ref Ch.4 (Design of on-Road facilities)
AASHTO Guide for the Development
of Bicycle Facilities, 2012

Intersection
bike lane with turn lane

LEFT TURN SECTION

N.T.S.

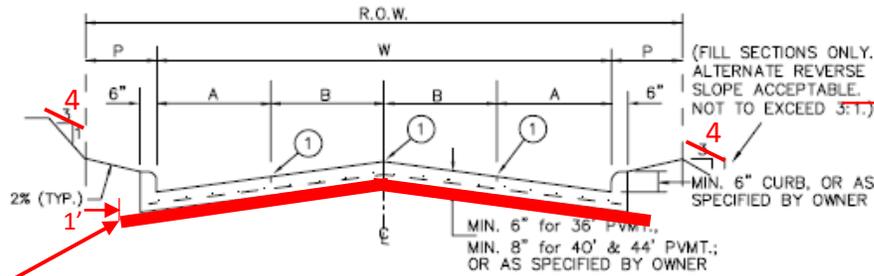


PLAN
N.T.S.

NOTES:

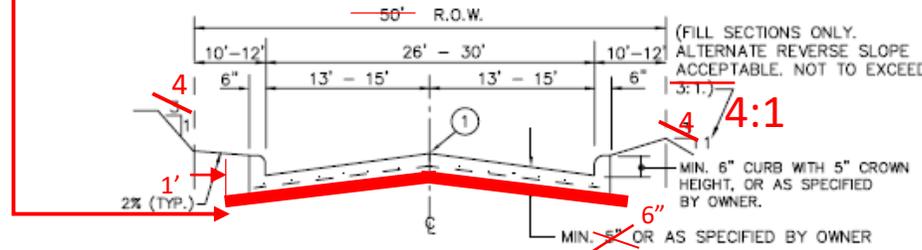
1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 8" - CLASS "C" OR "PC", OR AS SPECIFIED BY OWNER.
2. MIN. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY OWNER.
3. ALTERNATE REINFORCEMENT SHALL BE #4 BARS ON 30" CENTERS BOTH WAYS.

Intersection Crossing markings varies
 ref. NACTO Urban Bikeway Design Guide (Intersections Chapter)
 ① SAWED LONGITUDINAL CONTRACTION JOINT OR CONSTRUCTION JOINT.



STREET WIDTH(W)	A	B	R.O.W. WIDTH	P	CROWN HEIGHT
36'	8'	10'	VARIES	VARIES	6"
40'	8' OR 10'	10' OR 12'	VARIES	VARIES	6"
44'	11'	11'	VARIES	VARIES	8"

FOUR TRAVEL LANES OR
TWO TRAVEL LANES & TWO PARKING LANES
N.T.S.



ONE TRAVEL LANE & TWO PARKING LANES
N.T.S.

① INDICATES SAWED LONGITUDINAL CONTRACTION OR CONSTRUCTION JOINT.

NOTES :

- ALL REINFORCEMENT SHALL BE ~~3~~⁴ BARS ON ~~30~~¹⁸ CENTERS BOTH WAYS, EXCEPT WHERE NOTED.
- ALTERNATE REINFORCEMENT SHALL BE #4 BARS ON 30" CENTERS BOTH WAYS.
- PAVEMENT STRENGTH SHALL CONFORM TO CLASS "C" OR "PC" CONCRETE, OR AS SPECIFIED BY THE OWNER.

- Straight crown or parabolic crown as approved by owner
- See detail 2170 for sidewalks

8" minimum stabilized subgrade per section 301 and as approved or specified by owner

2. Alternative subgrade, thickness, and steel may be utilized with more detailed study and analysis and as approved by owner

REINFORCED CONCRETE PAVEMENT

2- & 4-LANE UNDIVIDED THOROUGHFARE

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

303

DATE

OCT. '04

STANDARD DRAWING NO.

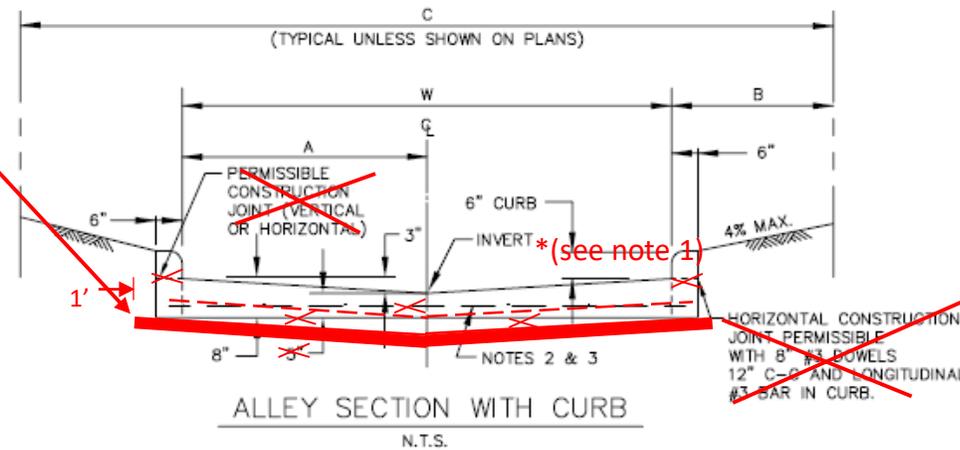
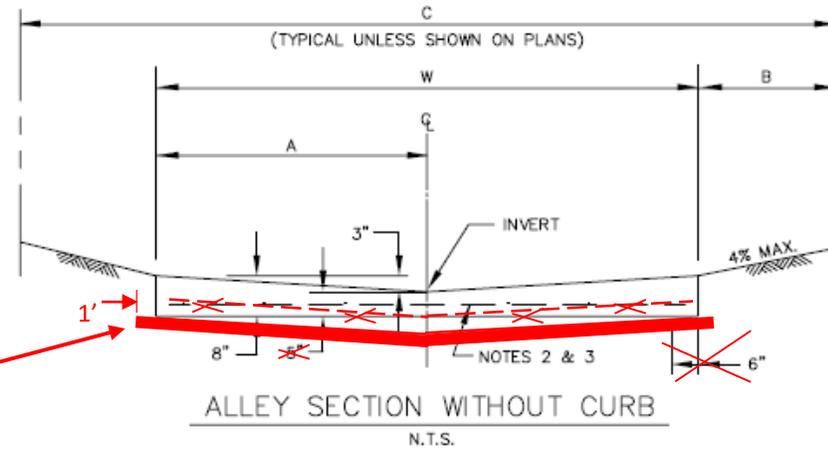
2030

8" minimum stabilized subgrade per section 301 and as approved or specified by owner

1. Crown section may be used in lieu of invert with provision of an adequate drainage design and as approved by owner

3. Alternative subgrade, thickness, and steel may be utilized with more detailed study and analysis and as approved by owner

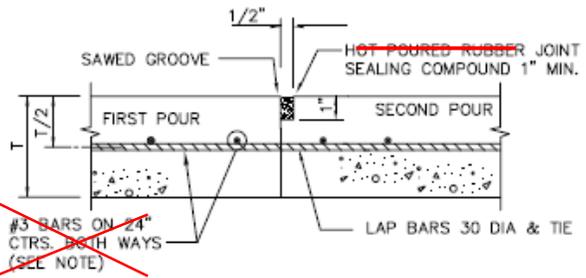
6. See detail 2170 for sidewalks



NOTES:

- ~~1. PROVIDE SAWS TRANSVERSE CONTRACTION JOINTS NOT MORE THAN 20' C-C. 18" OR AS APPROVED BY OWNER~~
- ~~2. REINFORCED WITH NO. 3 BARS AT 24" C-C BOTH WAYS.~~
- ~~3. ALTERNATE REINFORCEMENT - NO. 4 BARS AT 36" C-C BOTH WAYS.~~
- EXPANSION JOINTS TO BE PLACED AT ALL INTERSECTIONS AND NOT TO EXCEED 600' BETWEEN JOINTS.
- CONCRETE SHALL BE CLASS "C" OR "PC", OR AS SPECIFIED BY OWNER.

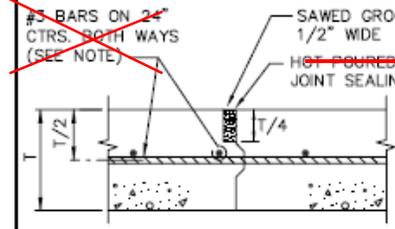
ALLEY WIDTH (W)	A	B	R.O.W. WIDTH (C)
10'	5'	2'-6"	15'
12'	6'	2'-6"	17'
16'	8'	2'-6"	21'
20'	10'	2'-6"	25'



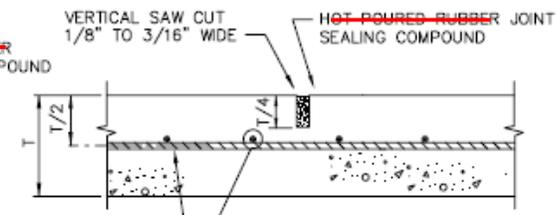
No. 4 Bars on 18" CTRS. Both ways

CONSTRUCTION JOINT
N.T.S.

No. 4 Bars on 18" CTRS. Both ways



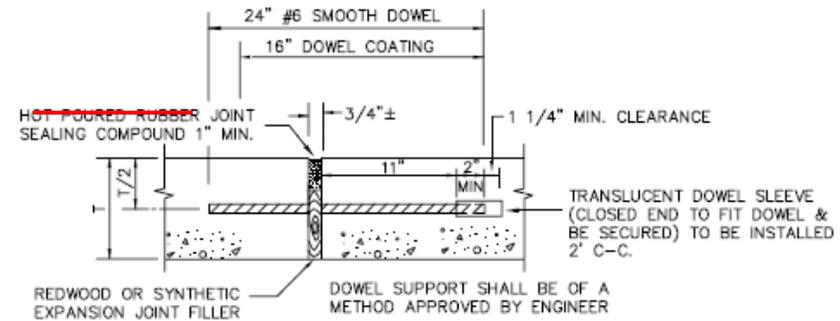
KEYWAY JOINT
(FOR PAVEMENT THICKNESS > 6")
N.T.S.



No. 4 Bars on 18" CTRS. Both ways
SAWED CONTRACTION JOINT
N.T.S.

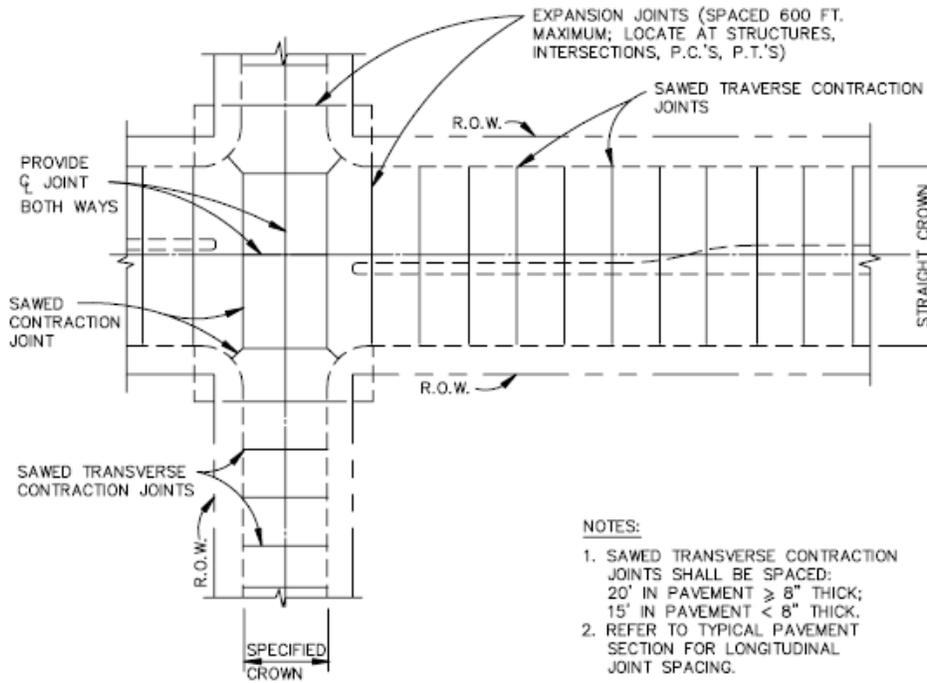
NOTE:
~~ALTERNATE REINFORCEMENT~~
~~#4 BARS ON 30" CTRS.~~
~~BOTH WAYS.~~

1. Apply backer rod as approved by owner



EXPANSION JOINT
(SPACED 600 FT. MAXIMUM; LOCATE AT STRUCTURES AND AT INTERSECTION P.C.'S & P.T.'S)
N.T.S.

REINFORCED CONCRETE PAVEMENT JOINTS	North Central Texas Council of Governments 	STANDARD SPECIFICATION REFERENCE 303.5.4.
	DATE OCT. '04	STANDARD DRAWING NO. 2050



*cleanup lines through median

NOTES:

1. SAWED TRAVERSE CONTRACTION JOINTS SHALL BE SPACED:
20' IN PAVEMENT \geq 8" THICK;
15' IN PAVEMENT $<$ 8" THICK.
2. REFER TO TYPICAL PAVEMENT SECTION FOR LONGITUDINAL JOINT SPACING.

SPACING DIAGRAM FOR TRANSVERSE JOINTS

N.T.S.

REINFORCED CONCRETE PAVEMENT

TRANSVERSE JOINT SPACING

North Central Texas Council of Governments

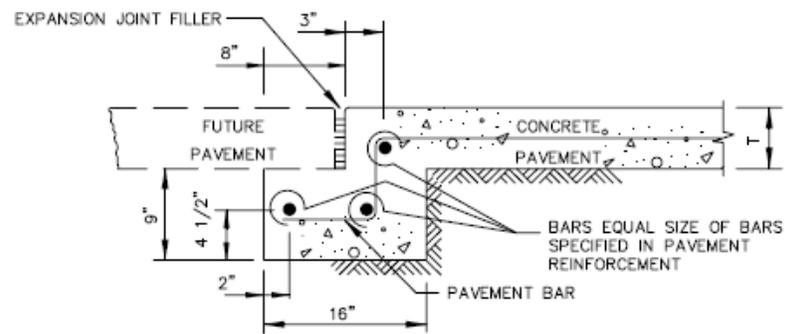


STANDARD SPECIFICATION REFERENCE

303.5.4.

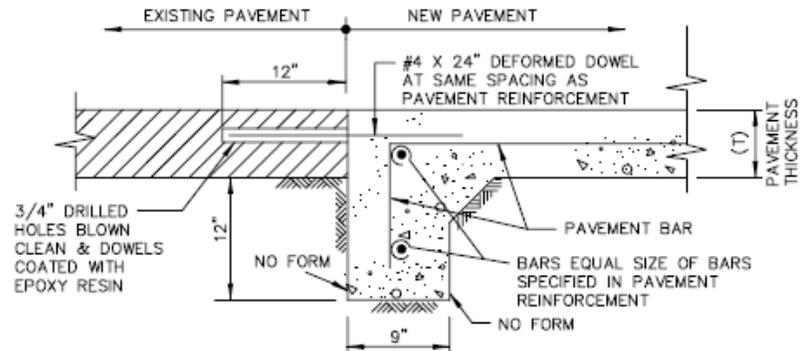
DATE
OCT. '04

STANDARD DRAWING NO.
2060



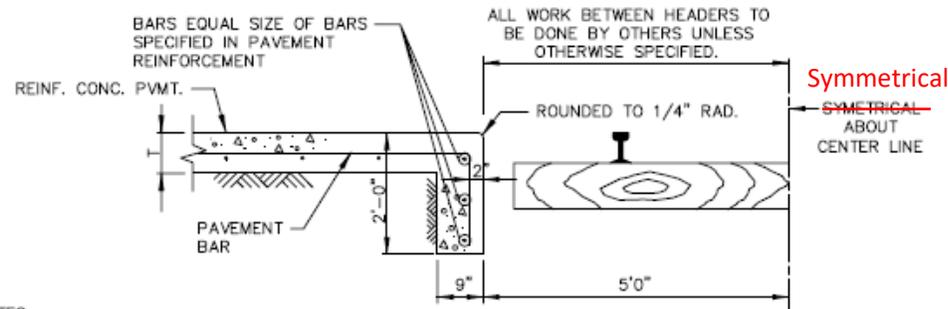
STREET HEADER FOR FUTURE PAVEMENT

N.T.S.



STREET HEADER AT EXISTING PAVEMENT

N.T.S.



STREET HEADER AT RAILROAD

N.T.S.

NOTES:

1. PAVEMENT BARS TO BE BENT DOWN INTO HEADER.
2. HEADER AND PAVEMENT TO BE MONOLITHIC.

REINFORCED CONCRETE PAVEMENT

STREET HEADERS

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

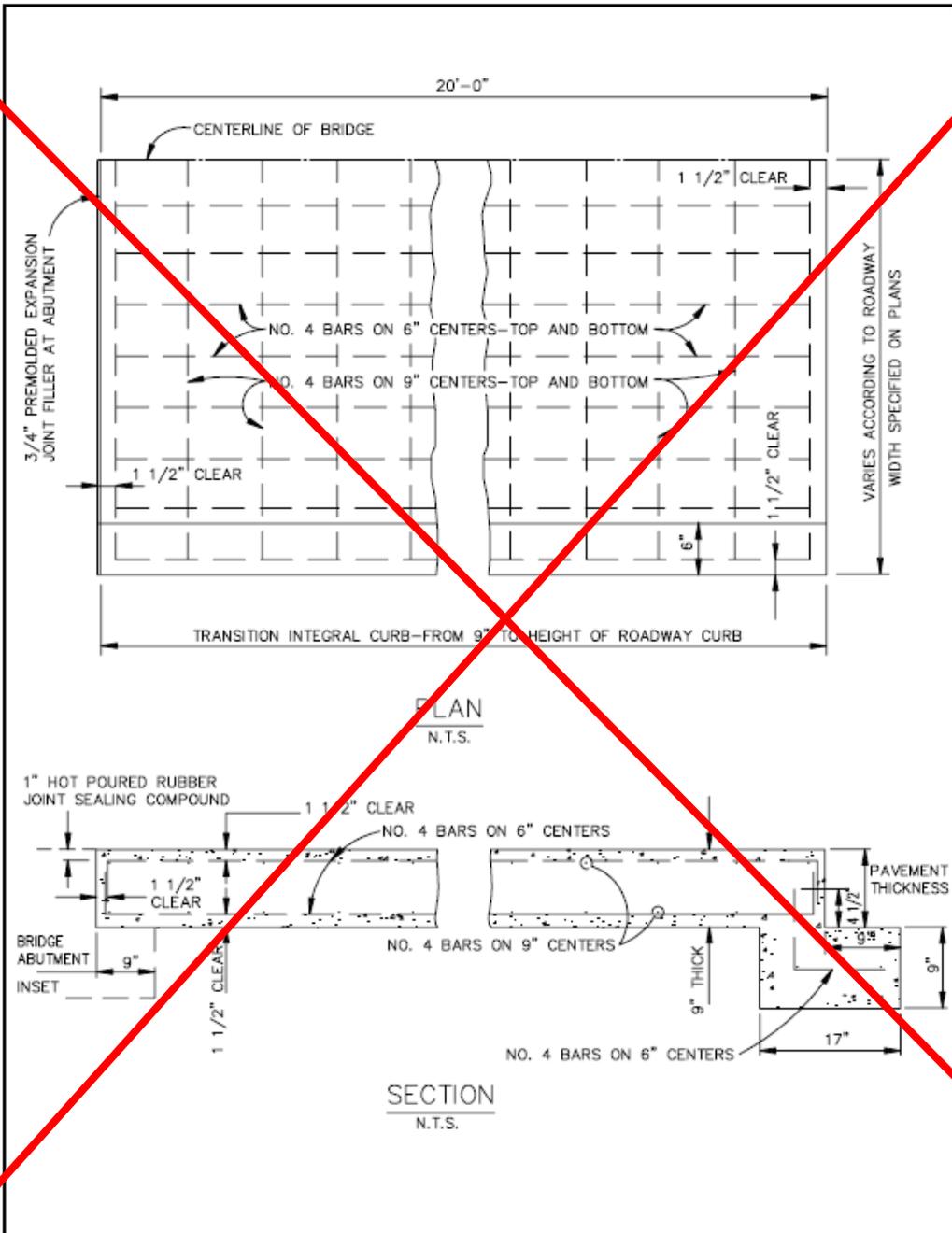
305.4

DATE

OCT. '04

STANDARD DRAWING NO.

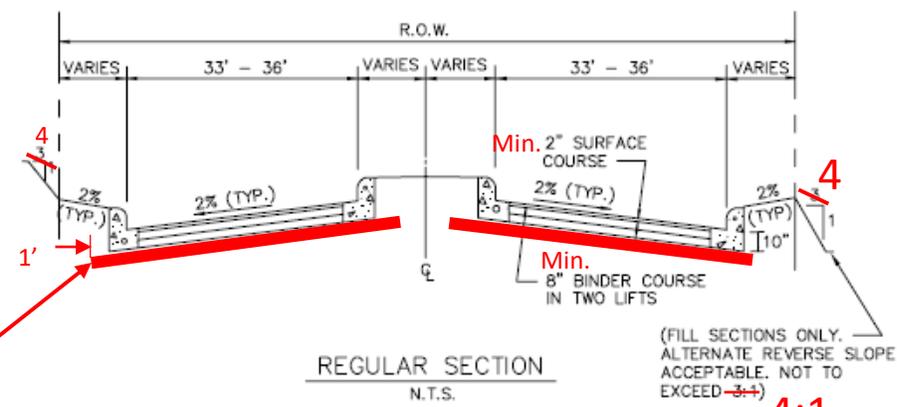
2070



Remove detail and reference TxDOT detail in specs if needed

REINFORCED CONCRETE PAVEMENT	North Central Texas Council of Governments	STANDARD SPECIFICATION REFERENCE 303
BRIDGE APPROACH SLAB		DATE OCT. '04 STANDARD DRAWING NO. 2080

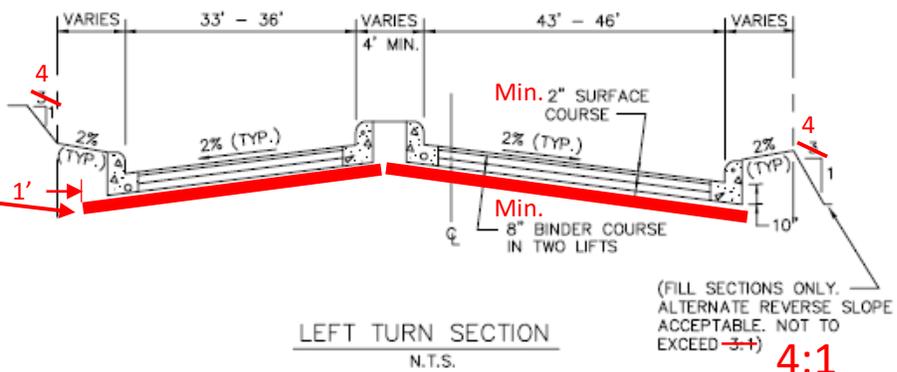
8" minimum stabilized subgrade per section 301 and as approved or specified by owner



REGULAR SECTION
N.T.S.

(FILL SECTIONS ONLY. ALTERNATE REVERSE SLOPE ACCEPTABLE. NOT TO EXCEED $\frac{4}{1}$)

4:1



LEFT TURN SECTION
N.T.S.

(FILL SECTIONS ONLY. ALTERNATE REVERSE SLOPE ACCEPTABLE. NOT TO EXCEED $\frac{4}{1}$)

4:1

MIN. PAVEMENT DEPTH = 10" $\left\{ \begin{array}{l} 2" \text{ HMA SURFACE COURSE} \\ 8" \text{ HMA BINDER COURSE} \end{array} \right.$

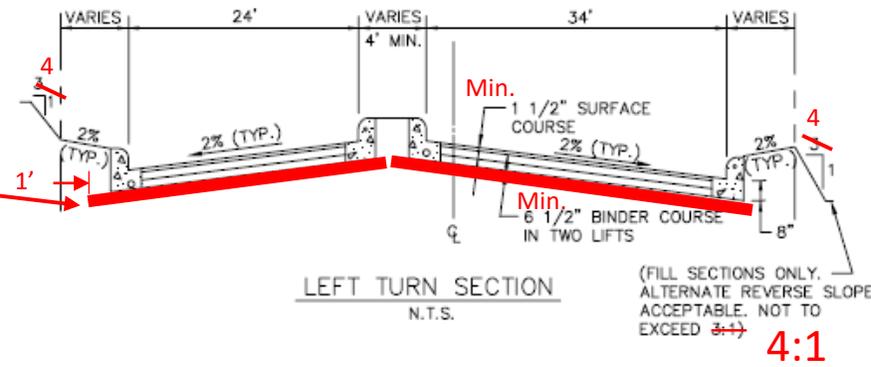
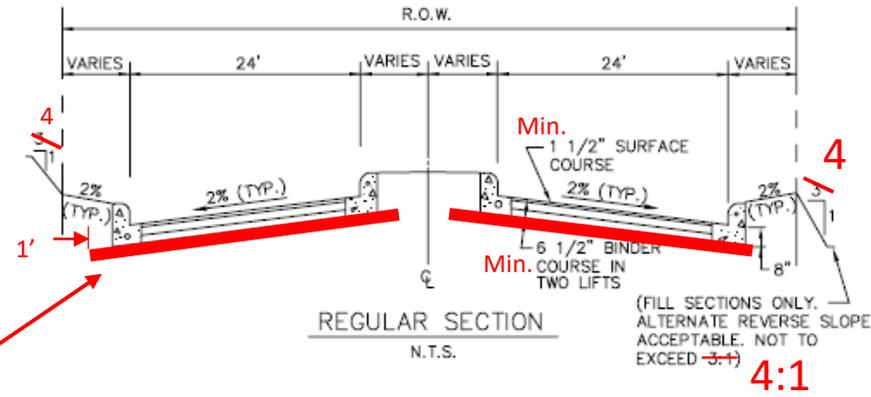
(SEE STANDARD DRAWING NO. 2010 FOR PLAN VIEW)

NOTES:

1. A SOIL INVESTIGATION FOR SUBGRADE DESIGN SHALL BE CONDUCTED BY THE ENGINEER AND THIS DESIGN SHALL BE APPROVED BY THE OWNER PRIOR TO CONSTRUCTION.
2. WHERE FULL-DEPTH ASPHALT PAVEMENTS ARE BEING CONSIDERED FOR USE, THE ASPHALT PAVEMENT THICKNESS SHALL BE BASED UPON NECESSARY SUBGRADE ANALYSES AND PAVEMENT THICKNESS DESIGN DETERMINATIONS AS APPROVED BY THE OWNER.
3. MIN. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY OWNER.
4. TACK COAT BETWEEN COURSES AS REQUIRED.

5. See detail 2170 for sidewalks

8" minimum stabilized subgrade per section 301 and as approved or specified by owner



MIN. PAVEMENT DEPTH = 8" { 1 1/2" HMA SURFACE COURSE
2-3 1/4" HMA BINDER COURSES

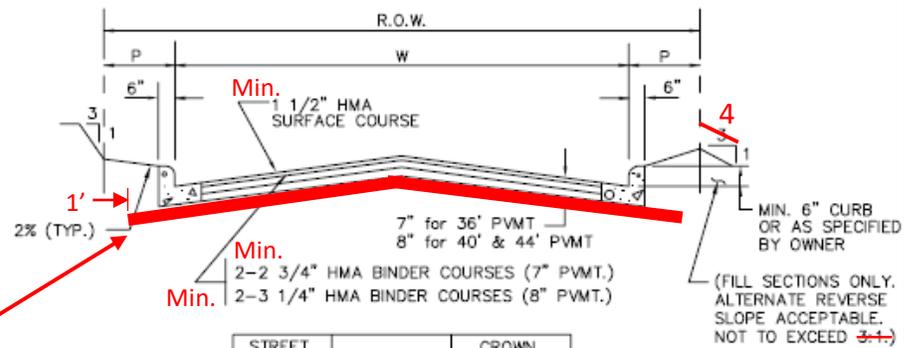
(SEE STANDARD DRAWING NO. 2020 FOR PLAN VIEW)

NOTES:

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5. See detail 2170 for sidewalks

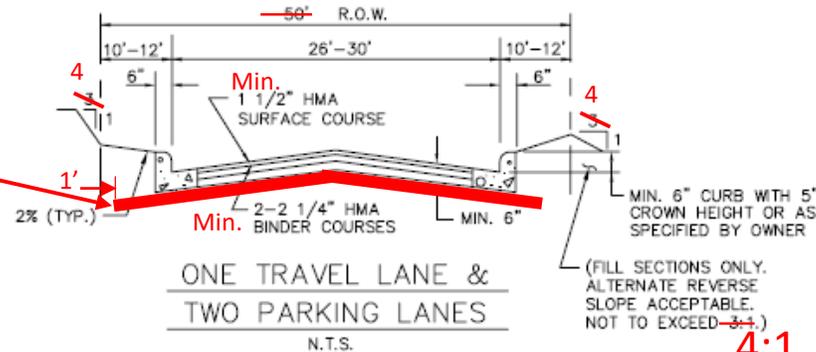




STREET WIDTH(W)	P	CROWN HEIGHT
36'	VARIES	6"
40'	VARIES	6"
44'	VARIES	8"

FOUR TRAVEL LANES OR
TWO TRAVEL LANES &
TWO PARKING LANES
N.T.S.

4:1



ONE TRAVEL LANE &
TWO PARKING LANES
N.T.S.

4:1

NOTES:

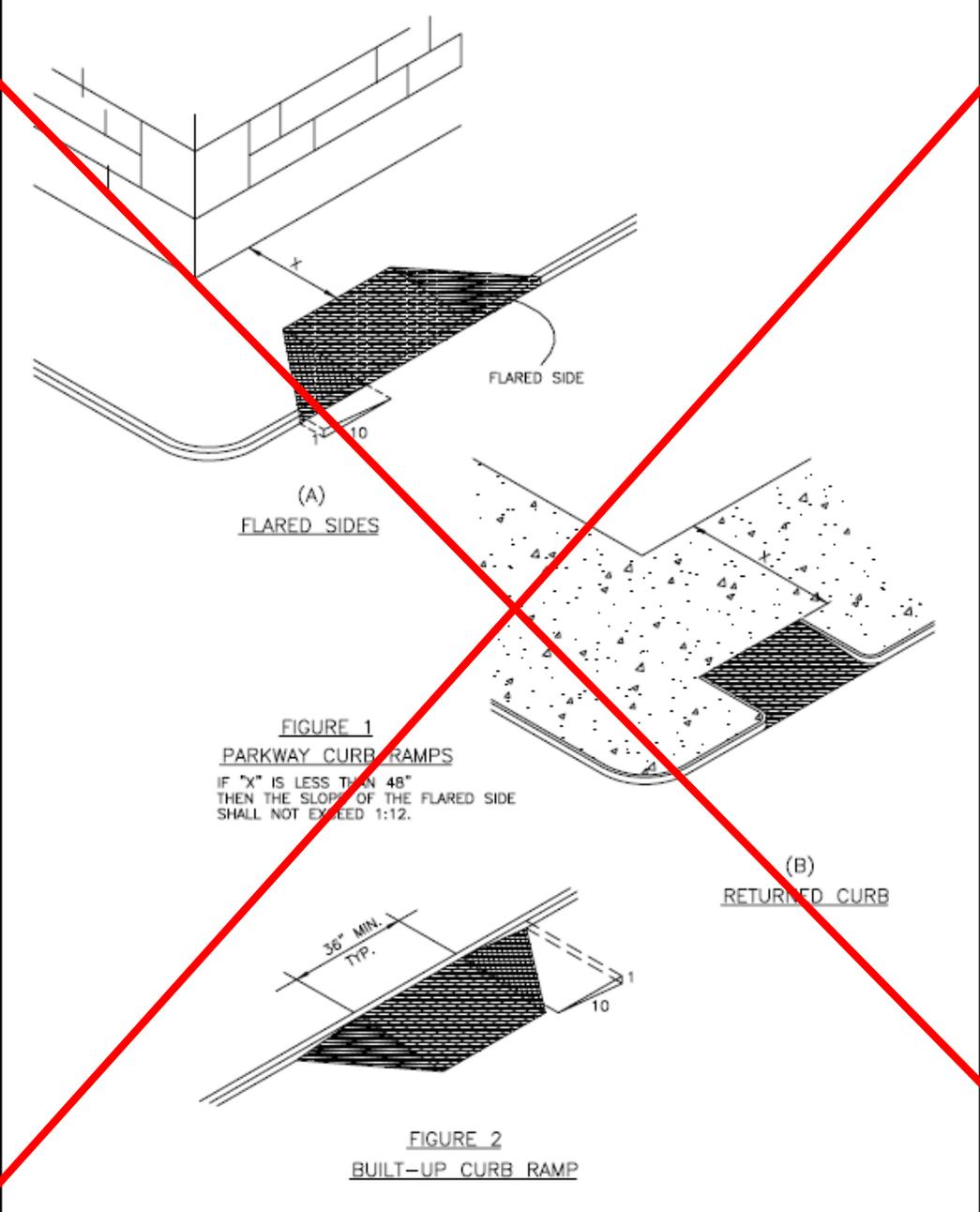
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2. WHERE FULL-DEPTH ASPHALT PAVEMENTS ARE BEING CONSIDERED FOR USE, THE ASPHALT PAVEMENT THICKNESS SHALL BE BASED UPON NECESSARY SUBGRADE ANALYSES AND PAVEMENT THICKNESS DESIGN DETERMINATIONS AS APPROVED BY THE OWNER. THICKNESSES SHOWN ARE TYPICAL.
3. TACK COAT BETWEEN COURSES AS REQUIRED.

4. Straight crown or parabolic crown as approved by owner

5. See detail 2170 for sidewalks

8" minimum stabilized subgrade per section 301 and as approved or specified by owner





(A)
FLARED SIDES

FIGURE 1
PARKWAY CURB RAMPS
IF "X" IS LESS THAN 48"
THEN THE SLOPE OF THE FLARED SIDE
SHALL NOT EXCEED 1:12.

(B)
RETURNED CURB

FIGURE 2
BUILT-UP CURB RAMP

reference TxDot

CURB RAMPS	North Central Texas Council of Governments	STANDARD SPECIFICATION REFERENCE	
		DATE OCT. '04	STANDARD DRAWING NO. 2125A

CURB RAMPS NOTES:

GENERAL REQUIREMENTS

CURB RAMPS SHALL BE CONSTRUCTED AS PER THE REQUIREMENTS AND SPECIFICATIONS OF THE TEXAS ACCESSIBILITY STANDARDS AND THE ADA & ABA ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES. (FEDERAL REGISTER/ VOL. 69, NO. 141, FRIDAY, JULY 23, 2004)

LOCATION:

CURB RAMPS UNDER THESE PROVISIONS, SHALL BE WHEREVER AN ACCESSIBLE ROUTE CROSSES A CURB.

SLOPE:

SLOPES ON CURB RAMPS SHALL BE MEASURED AS FOLLOWS: (Y:X = VERTICAL:HORIZONTAL)

- A) TRANSITIONS FROM RAMPS TO WALKS, GUTTERS, OR STREETS SHALL BE FLUSH AND FREE OF ABRUPT CHANGES.
- B) MAXIMUM SLOPES OF ADJOINING GUTTERS, ROAD SURFACE IMMEDIATELY ADJACENT TO THE CURB OR ACCESSIBLE ROUTE SHALL NOT EXCEED 1:20.
- C) THE LEAST POSSIBLE SLOPE SHALL BE USED FOR ANY RAMP. THE MAXIMUM SLOPE OF A RAMP IN NEW CONSTRUCTION SHALL BE 1:12. THE MAXIMUM RISE FOR ANY RUN SHALL BE 30" (760 MM). CURB RAMPS AND RAMPS TO BE CONSTRUCTED ON EXISTING SITES OR IN EXISTING BUILDINGS OR FACILITIES MAY HAVE SLOPES AND RISES IF SPACE LIMITATIONS PROHIBIT THE USE OF A 1:12 SLOPE OR LESS, AS FOLLOWS:
 - 1. A SLOPE BETWEEN 1:10 AND 1:12 IS ALLOWED FOR A MAXIMUM RISE OF 6".
 - 2. A SLOPE BETWEEN 1:8 AND 1:10 IS ALLOWED FOR A MAXIMUM OF 3" A SLOPE STEEPER THAN 1:8 IS NOT ALLOWED.

RAMP WIDTH:

THE MINIMUM WIDTH OF A CURB RAMP SHALL BE 36" EXCLUSIVE OF FLARED SIDES.

SURFACE:

SURFACES OF CURB RAMPS, SHALL BE STABLE FIRM, AND SLIP RESISTANT. SURFACE TEXTURES SHALL CONSIST OF EXPOSED CRUSHED STONE AGGREGATE, ROUGHENED CONCRETE, RUBBER, RAISED ABRASIVE STRIPS, OR GROOVES. EXTENDING THE FULL WIDTH AND DEPTH OF THE CURB RAMP. SURFACES THAT ARE RAISED, ETCHED, OR GROOVED IN A WAY THAT WOULD ALLOW WATER TO ACCUMULATE ARE PROHIBITED. FOR PURPOSES OF WARNING, THE FULL WIDTH AND DEPTH OF CURB RAMPS SHALL HAVE A LIGHT REFLECTIVE VALUE AND TEXTURE THAT SIGNIFICANTLY CONTRASTS WITH THAT OF ADJOINING PEDESTRIAN ROUTES.

SIDES OF CURB RAMPS:

IF A CURB RAMP IS LOCATED WHERE PEDESTRIANS MUST WALK ACROSS THE RAMP, OR WHERE IT IS NOT PROTECTED BY HANDRAILS OR GUARDRAILS, IT SHALL HAVE FLARED SIDES. THE MAXIMUM SLOPE OF THE FLARE SHALL BE 1:10 (SEE FIG. 1 (A)) CURB RAMPS WITH RETURNED CURBS MAY BE USED WHERE PEDESTRIANS WOULD NOT WALK ACROSS THE RAMP. (SEE FIG. 1 (B))

BUILT-UP RAMPS:

BUILT-UP CURB RAMPS SHALL BE LOCATED SO THEY DO NOT PROJECT INTO VEHICULAR TRAFFIC LANES (SEE FIG. 2)

OBSTRUCTIONS:

CURB RAMPS SHALL BE LOCATED OR PROTECTED TO PREVENT THEIR OBSTRUCTION BY PARKED VEHICLES.

LOCATION AT MARKED CROSSINGS:

CURB RAMPS AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED SIDES.

DIAGONAL CURB RAMPS:

IF DIAGONAL (OR CORNER TYPE) CURB RAMPS HAVE RETURNED CURBS OR OTHER WELL DEFINED EDGES, SUCH EDGES SHALL BE PARALLEL TO THE DIRECTION OF PEDESTRIAN FLOW. THE BOTTOM OF DIAGONAL CURB RAMPS SHALL HAVE 48" (1220 MM) MINIMUM. IF DIAGONAL CURB RAMPS ARE PROVIDED AT MARKED CROSSINGS, THE 48" (1220 MM) CLEAR SPACE SHALL BE WITHIN THE MARKINGS. IF DIAGONAL CURB RAMPS HAVE FLARED SIDES, THEY SHALL ALSO HAVE AT LEAST A 24" (610 MM) LONG SEGMENT OF STRAIGHT CURB LOCATED ON EACH SIDE OF THE CURB RAMP AND WITHIN THE MARKED CROSSING. ANY RAISED ISLANDS IN CROSSINGS SHALL BE CUT THROUGH LEVEL WITH THE STREET OR HAVE CURB RAMPS AT BOTH SIDES AND A LEVEL AREA AT LEAST 48" (1220 MM) LONG BETWEEN THE CURB RAMPS IN THE PART OF THE ISLAND INTERSECTED BY THE CROSSINGS.

CONSTRUCTION

- (A.) THE CONTRACTOR SHALL SAWCUT, REMOVE AND DISPOSE OFF-SITE THE REQUIRED EXISTING CONCRETE SIDEWALK, CURB AND GUTTER, TO CONSTRUCT THE PROPOSED RAMPS.
- (B.) CONCRETE SIDEWALKS AND RAMPS SHALL BE MINIMUM 4" THICK, 4000 PSI, 5 SACK CONCRETE, REINFORCED WITH #3 BARS AT 14" CENTERS BOTH WAYS, PLACED OVER A 2" THICK SAND CUSHION EMBEDMENT.
- (C.) THE CONTRACTOR SHALL USE 1" PREMOLDED EXPANSION JOINT MATERIAL BETWEEN THE PROPOSED SIDEWALKS AND RAMPS AT THE BACK OF CURBS, AND AT JOINTS AT NO EXTRA PAY.
- (D.) DUMMY JOINT REQUIRED EVERY 4' IN 4' WIDE SIDEWALKS AND EVERY 5' IN 6' WIDE SIDEWALK.

Update and remove 2125A or update to reference other detail

CURB RAMPS

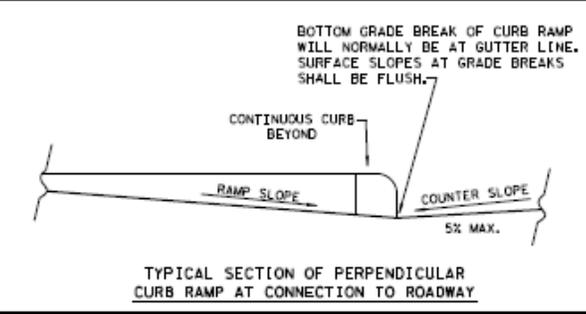
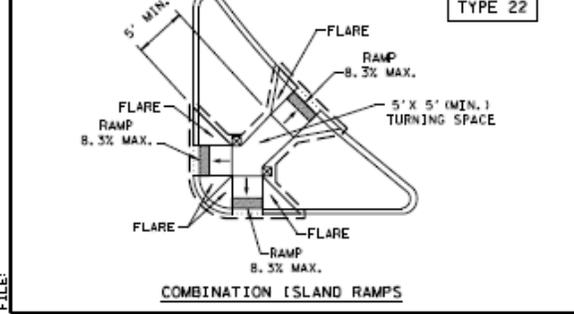
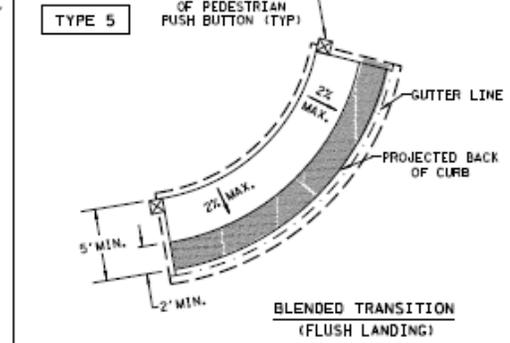
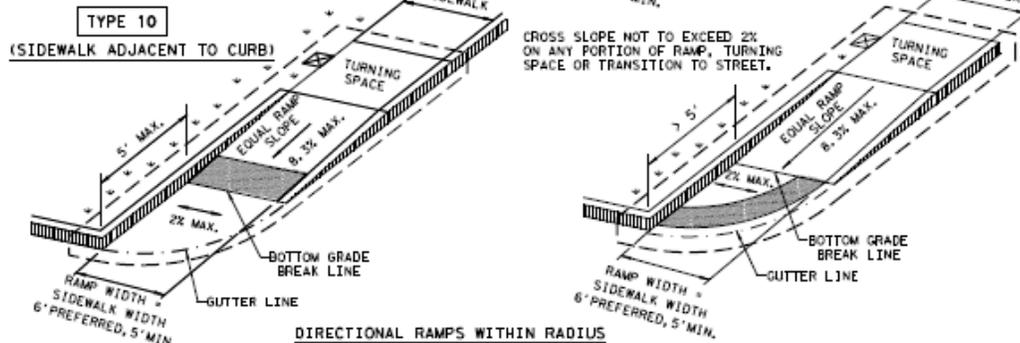
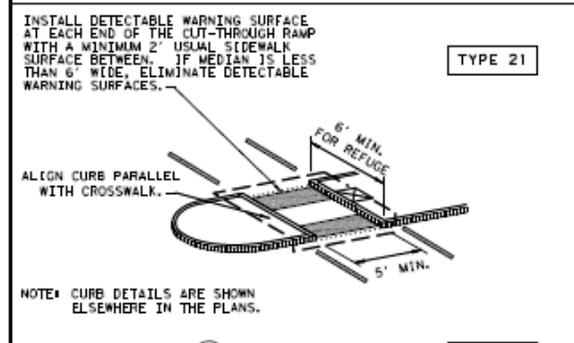
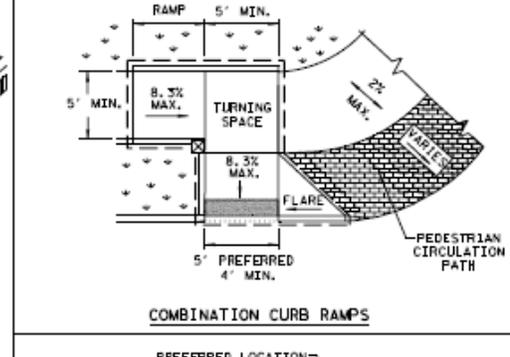
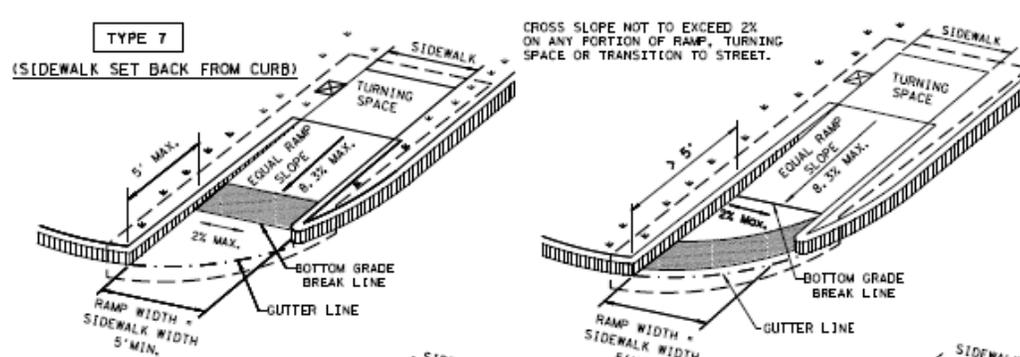
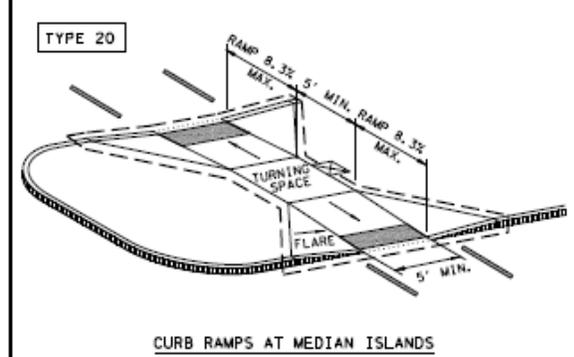
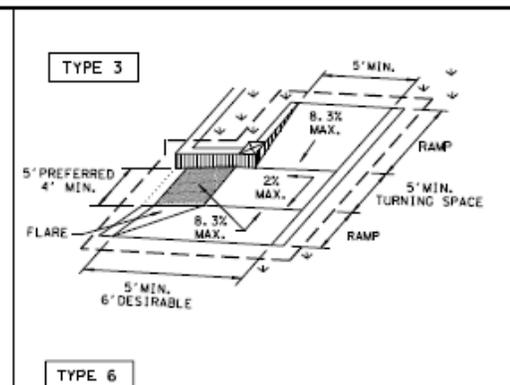
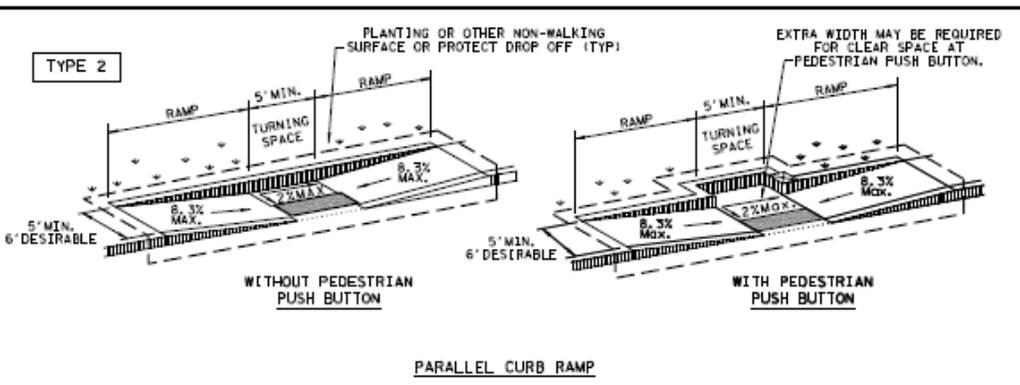
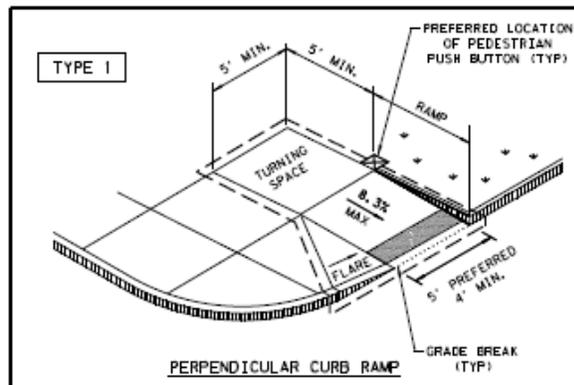
North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

DATE
OCT. '04

STANDARD DRAWING NO.
2125B



NOTES / LEGEND:

SEE GENERAL NOTES ON SHEET 2 OF 4 FOR MORE INFORMATION.

DENOTES PLANTING OR NON-WALKING SURFACE NOT PART OF PEDESTRIAN CIRCULATION PATH.

DETECTABLE WARNING SURFACE

DENOTES PREFERRED LOCATION OF PEDESTRIAN PUSH BUTTON IF APPLICABLE.

GUTTER LINE

GRADE BREAK

RAMP LIMITS OF PAYMENT

SHEET 1 OF 4

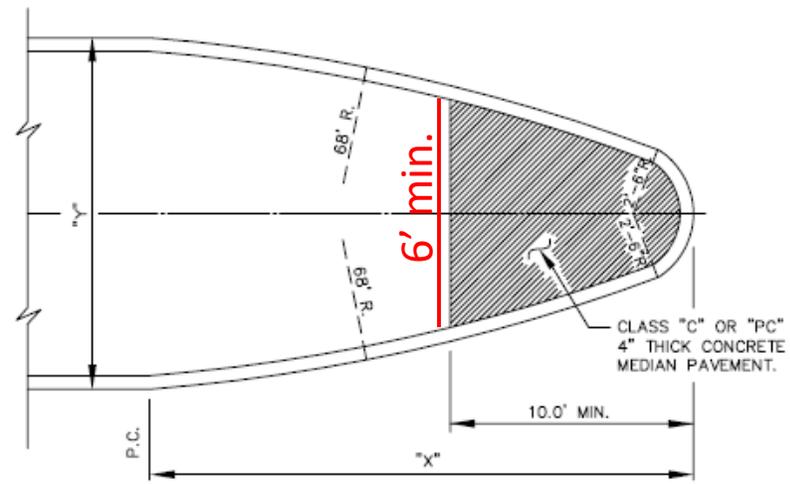
Texas Department of Transportation Design Division Standard

PEDESTRIAN FACILITIES CURB RAMPS

PED-18

FILE: ped18	REVISED	DATE	BY	CHKD	APP'D
© TxDOT MARCH, 2002	CONT	TEXT	ARE		10/19/02
REVISIONS					
REVISED 06, 2005					
REVISED 06, 2011					
REVISED 11, 2014					

FILE:



DIMENSIONS OF MEDIAN NOSE

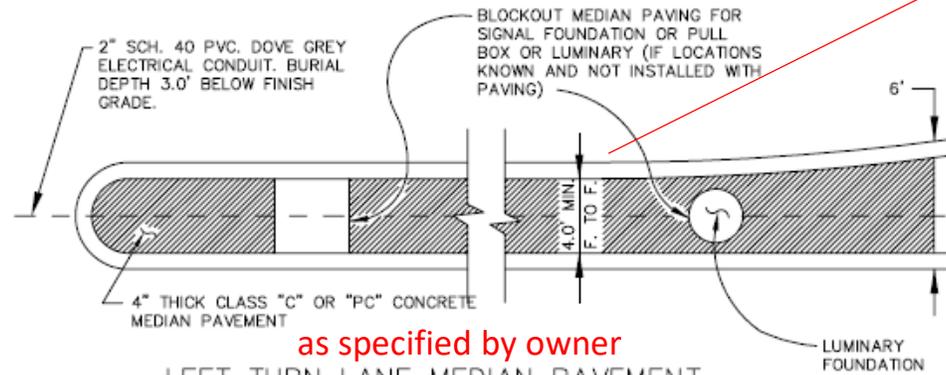
Y = 15'	X = 27.6'
Y = 16'	X = 28.8'
Y = 17'	X = 29.9'
Y = 18'	X = 30.9'

CONCRETE NOSE FOR MEDIAN ISLAND

N.T.S.

NOTE:

MEDIAN PAVING SHALL EXTEND TO POINT WHERE MEDIAN IS 6' WIDE. IF MEDIAN IS 6' WIDE, PAVING SHALL EXTEND 15' FROM NOSE. FOR MEDIANS WIDER THAN 6' PAVING SHALL EXTEND 10' FROM NOSE. ALL DISTANCES ARE MINIMUM.



as specified by owner

LEFT TURN LANE MEDIAN PAVEMENT

N.T.S.

Move arrows to outside lines or change to "B. to B."

Move arrows to inside lines

MEDIAN ISLAND PAVEMENT

NOSE & LEFT TURN LANE

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

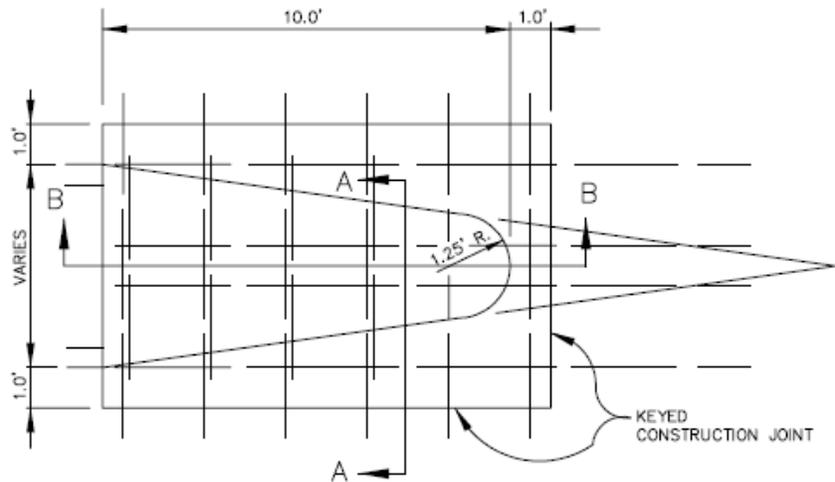
305.3

DATE

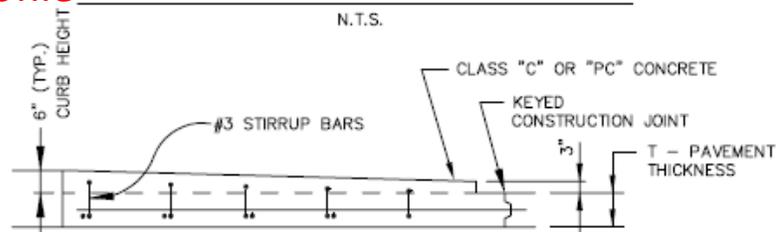
OCT. '04

STANDARD DRAWING NO.

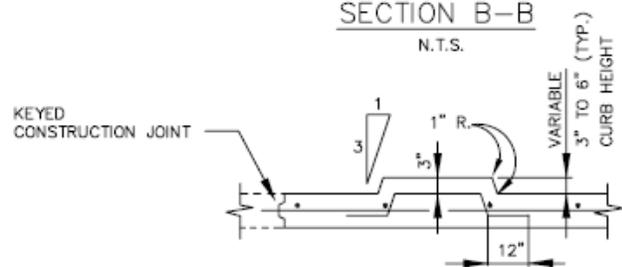
2130



Low profile MONOLITHIC CONCRETE MEDIAN NOSE
N.T.S.



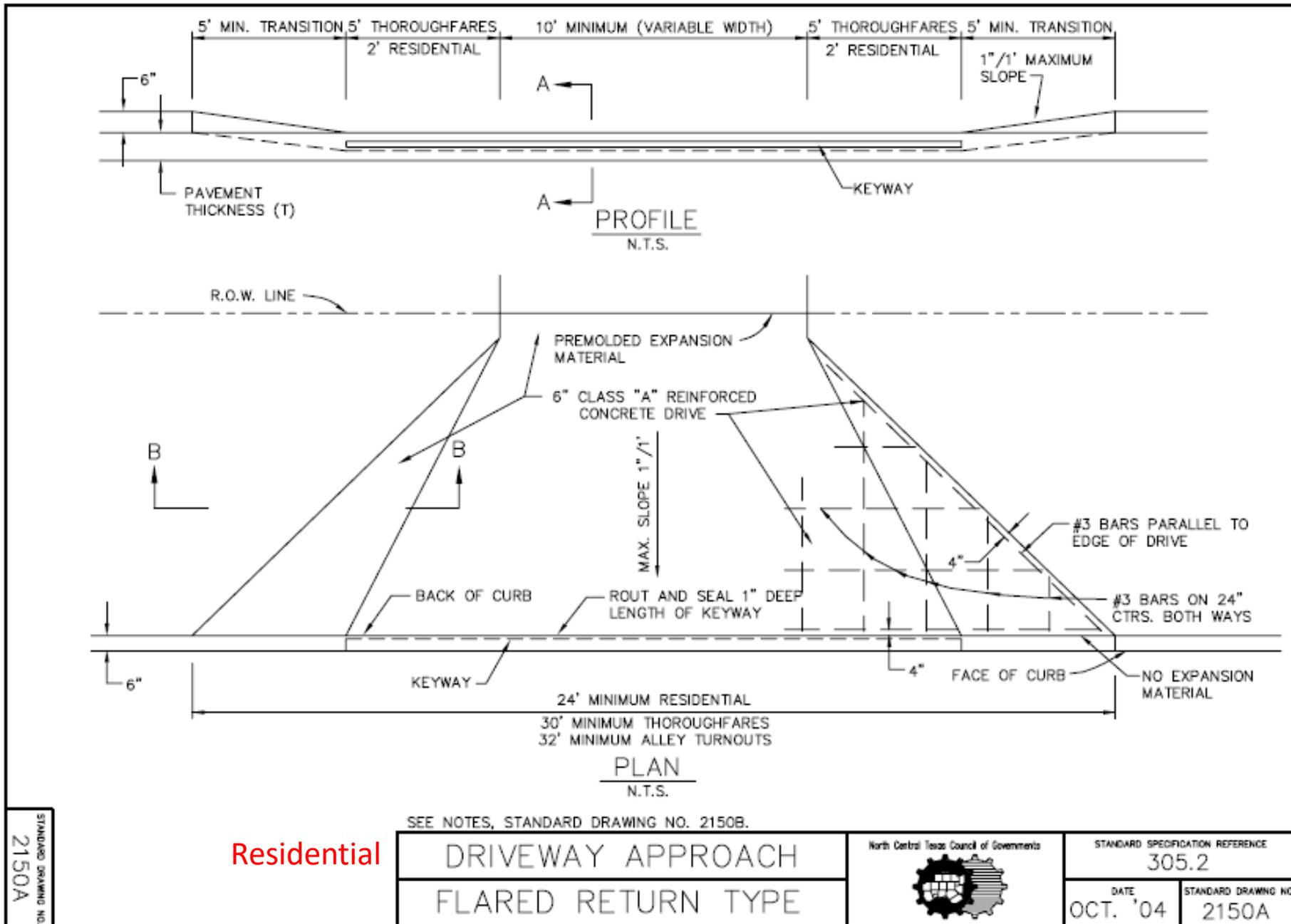
SECTION B-B
N.T.S.



SECTION A-A
N.T.S.

As specified
by owner

NOTE:
REINFORCEMENT BARS SHALL
MATCH THOSE IN PAVEMENT.



Eliminate keyway joint

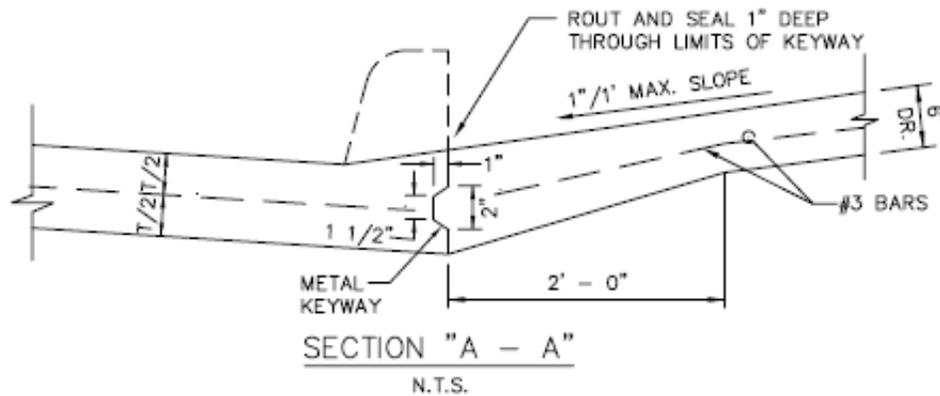
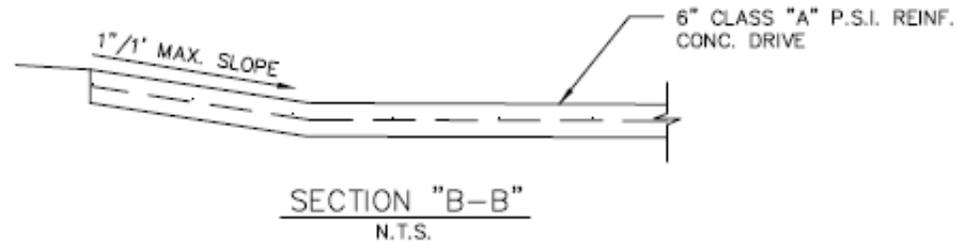
Update 2150, 2155, 2160 and to include residential, commercial and alley approach

Show doweling as option

Residential

DRIVEWAY APPROACH
FLARED RETURN TYPE





NOTES:

1. OFFSETS IN DRIVES TO MATCH PROPOSED WALKS SHALL BE BUILT MONOLITHIC WITH THE DRIVE.
2. PAVEMENT JOINTS SHALL NOT EXTEND THROUGH DRIVE.
3. KEYWAY LIMITS SHALL COINCIDE WITH LIMITS OF 1" CURB.
4. REINFORCING STEEL SHALL NOT EXTEND THROUGH KEYWAY. DRIVE SHALL NOT BE TIED TO PAVEMENT.
5. MAXIMUM SLOPE ON DRIVE IN ANY DIRECTION SHOULD BE 1" / 1', WITH EXCEPTION OF 1/4" / 1' THROUGH ANY SIDEWALK PASSTHROUGH, TO RESPECT PRINCIPLES OF BARRIER FREE CONSTRUCTION.
6. LENGTH OF TRANSITION FOR CURB AT EACH SIDE OF DRIVE MAY VARY DUE TO STREET GRADES AND REQUIREMENT TO HOLD MAXIMUM SLOPE OF 1" / 1'.
7. SIDEWALKS SHALL BE AS DIRECTED BY OWNER AND SHALL MEET REQUIREMENTS OF A.D.A.
8. EXTEND TRANSVERSE PAVEMENT SAWED JOINTS TO R.O.W.

Update
2150, 2155,
2160 and to
include
residential,
commercial
and alley
approach

STANDARD DRAWING NO.
2150B

Residential

DRIVEWAY APPROACH
FLARED RETURN TYPE

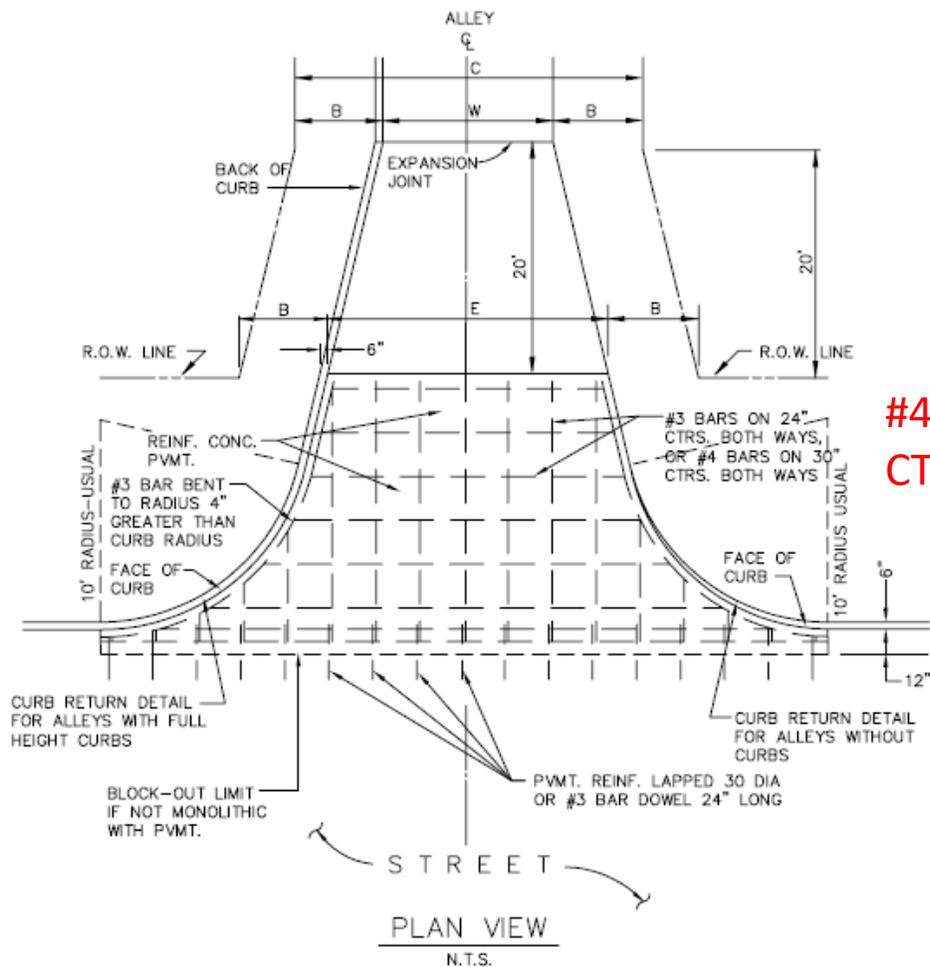
North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE
305.2

DATE
OCT. '04

STANDARD DRAWING NO.
2150B



#4 Bars on 18"
CTRS?

Update
2150, 2155,
2160 and to
include
residential,
commercial
and alley
approach

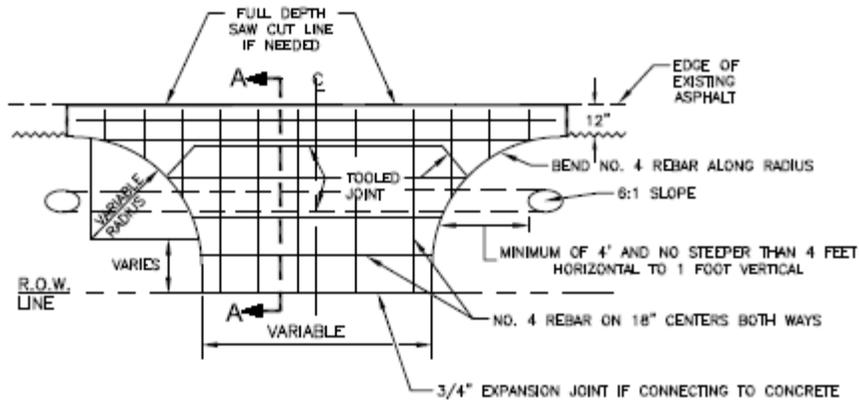
Over 12% grade break is
not recommended?

ALLEY WIDTH (W)	R.O.W. WIDTH (C)	B	E
10'	15'	2' - 6"	12'
12'	17'	2' - 6"	14'
16'	21'	2' - 6"	18'
20'	25'	2' - 6"	22'

Commercial
Driveway

ALLEY APPROACH RADIUS RETURN TYPE	North Central Texas Council of Governments 	STANDARD SPECIFICATION REFERENCE 305.2	
		DATE OCT. '04	STANDARD DRAWING NO. 2160

Arlington's Driveway Approaches



TYPICAL DRIVE APPROACH CONNECTING
TO EXISTING RURAL TYPE
ASPHALT STREETS

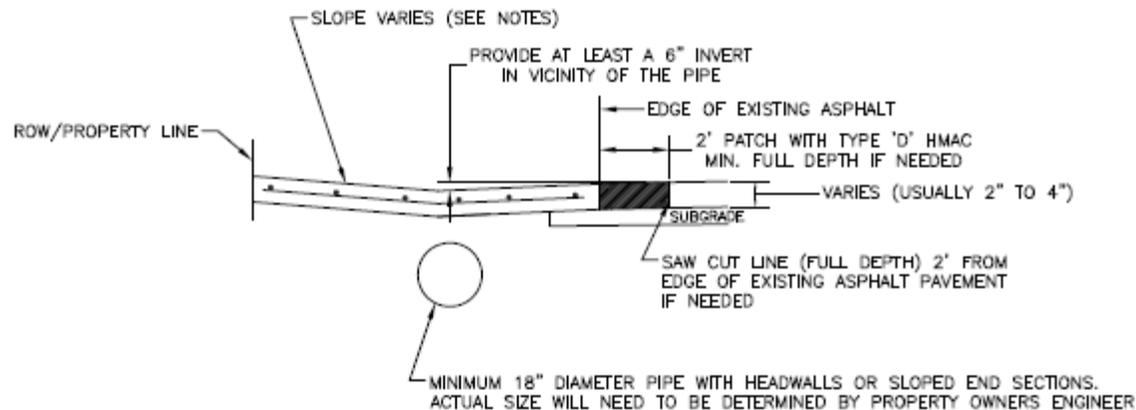
NTS REV: 8/22/17

NOTES:

1. THE SLOPE OF THE DRIVE WHERE SIDEWALKS CROSS SHALL HAVE A MAXIMUM CROSS SLOPE OF 2%.
2.

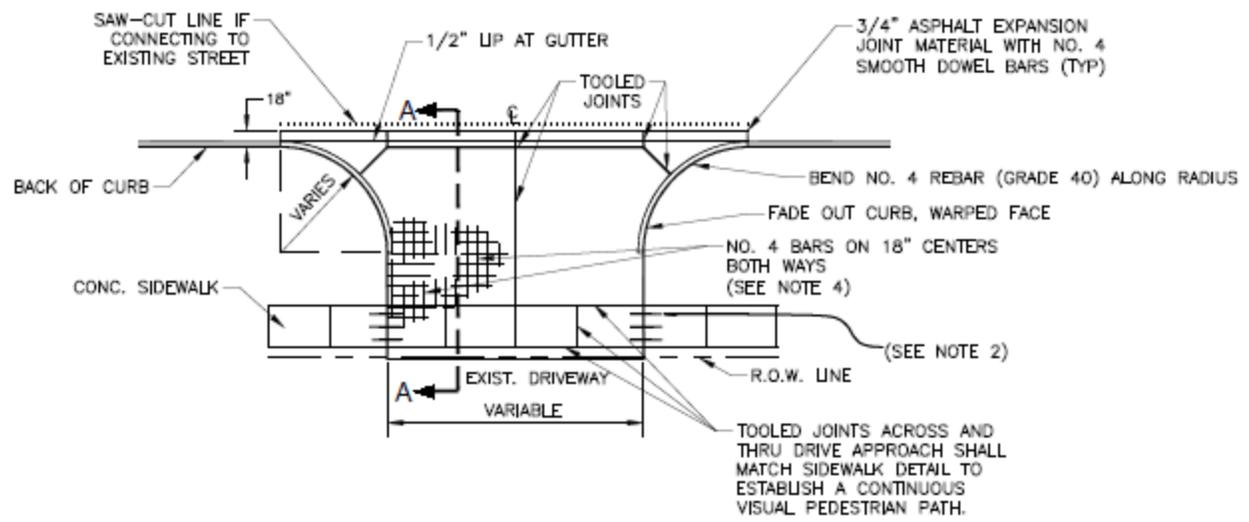
RESIDENTIAL	SLOPE (MAX)*	SLAB THICKNESS
	6%	5"
ALL OTHERS	3%	6"
- * MAXIMUM SLOPE DESIGNATED FOR NEW DEVELOPMENT CONSTRUCTION ONLY AND DOES NOT APPLY TO CAPITAL IMPROVEMENT RECONSTRUCTION OR REBUILD PROJECTS.

(ALSO SEE THE DESIGN CRITERIA MANUAL FOR OTHER SPECIFIC CRITERIA.)
3. ALL CONNECTIONS TO STATE RIGHT-OF-WAY SHALL USE TXDOT DETAILS.
4. CONCRETE SHALL BE CLASS C, 5 1/2 SACK AND HAVE COMPRESSIVE STRENGTH OF 3600 PSI @ 28 DAYS.
5. MINIMUM VELOCITY THROUGH PIPE IS 2.5fps. MINIMUM SLOPE IN PIPE IS 0.5% UNLESS OTHERWISE DESIGNED TO MEET MINIMUM SLOPE REQUIREMENTS.
6. IN SOME CASES A SWALE MAY BE PROVIDED IN LIEU OF THE PIPE. THE PROPERTY OWNER AND OWNER'S ENGINEER WILL NEED TO DETERMINE IF A SWALE CAN BE USED IN LIEU OF A PIPE.

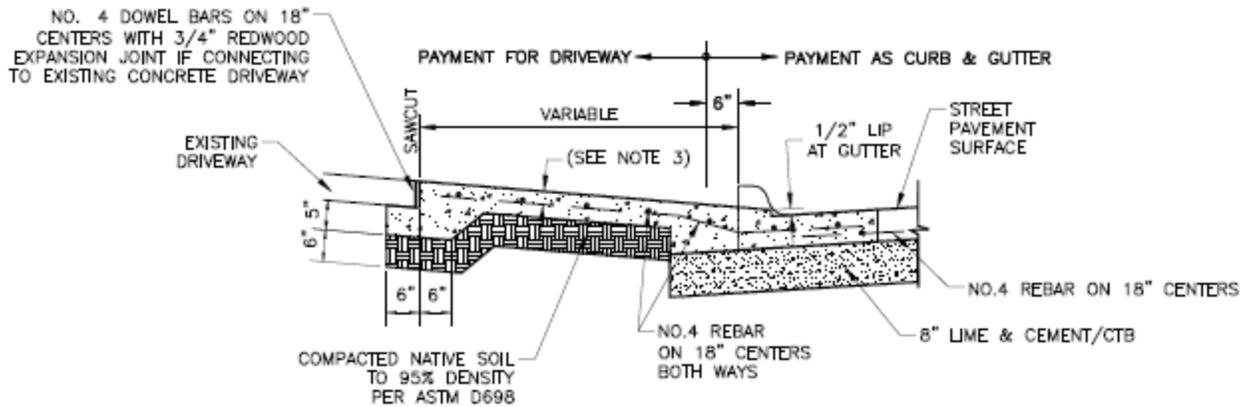


SECTION A-A
NTS

CITY OF ARLINGTON, TEXAS		
TYPICAL DRIVE APPROACH CONNECTING TO EXISTING RURAL TYPE ASPHALT STREETS		
DATE	SCALE: NTS	SHEET OF
DESIGNED BY:	DRAWN BY:	CHECKED BY:



TYPICAL DRIVE APPROACH
ON A CONCRETE STREET
NTS REV: 1/17/20



SECTION 'A-A'
NTS

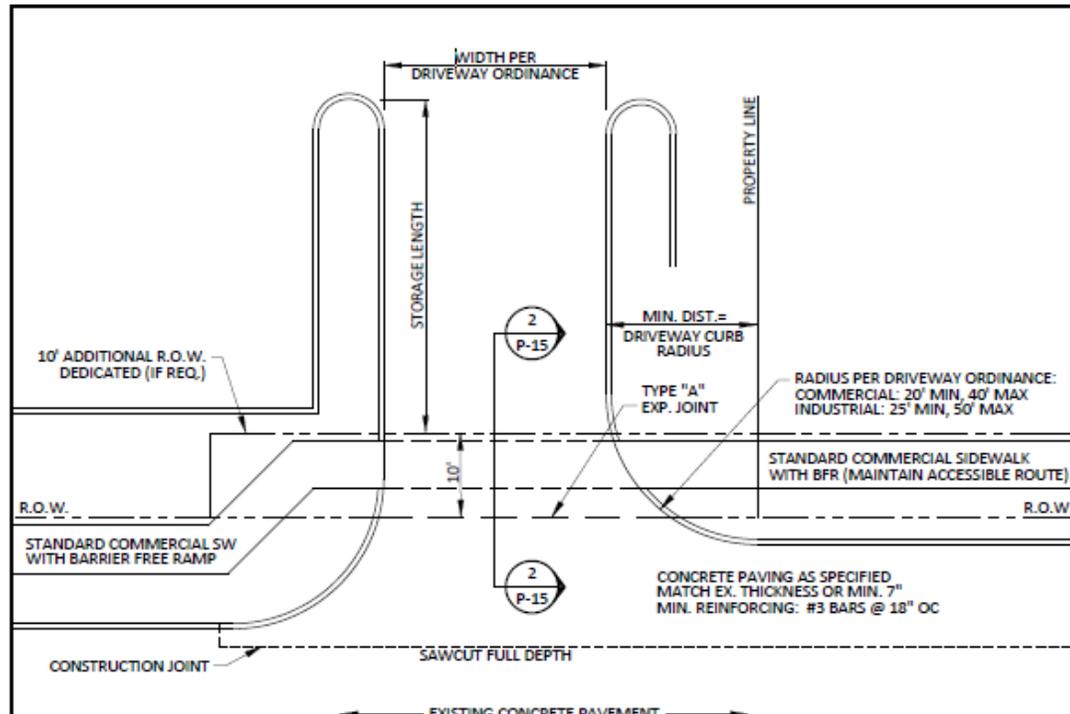
NOTES:

1. THE SLOPE OF THE DRIVE WHERE SIDEWALKS CROSS SHALL HAVE A MAXIMUM CROSS SLOPE OF 2%.
2. REMOVE ANY EXISTING SIDEWALK AT NEAREST JOINT AND CONNECT REPLACED SECTION TO DRIVE WITH (3) - NO. 4 SMOOTH DOWEL BARS ON 18" CENTERS WITH 3/4" REDWOOD EXPANSION JOINT WITH 1" REMOVABLE CAP STRIP. SEAL WITH SELF LEVELING GRAY SILICONE SEALANT.
3.

	SLOPE (MAX)	SLAB THICKNESS
RESIDENTIAL	6%	5"
ALL OTHERS	3%	6"
- * MAXIMUM SLOPE DESIGNATED FOR NEW DEVELOPMENT CONSTRUCTION ONLY AND DOES NOT APPLY TO CAPITAL IMPROVEMENT RECONSTRUCTION OR REBUILD PROJECTS.
(ALSO SEE THE DESIGN CRITERIA MANUAL FOR OTHER SPECIFIC CRITERIA.)
4. ALL CONNECTIONS TO STATE RIGHT-OF-WAY SHALL USE TxDOT DETAILS.
5. FOR PAYMENT, MEASUREMENT FOR DRIVEWAY QUANTITY BEGINS 6" FROM BACK OF CURB.
6. CONCRETE SHALL BE CLASS C, 5 1/2 SACK AND HAVE COMPRESSIVE STRENGTH OF 3600 PSI @ 28 DAYS.
7. IF CONSTRUCTING A DRIVEWAY ON AN EXISTING CONCRETE STREET, SAW-CUT (FULL DEPTH) AND CONNECT WITH EPOXY TIE BAR BUTT JOINT.
8. ALL REDWOOD EXPANSION JOINTS SHALL BE SEALED WITH SELF LEVELING GRAY SILICONE SEALANT.

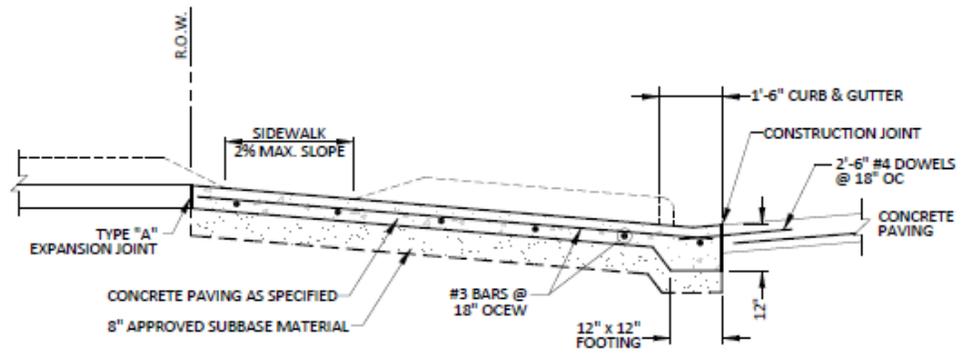
CITY OF ARLINGTON, TEXAS		
TYPICAL DRIVE APPROACH ON A CONCRETE STREET		
DATE:	SCALE: FTS	SHEET OF
DESIGNED BY:	DRAWN BY:	CHECKED BY:

Carrollton's Driveway Approaches

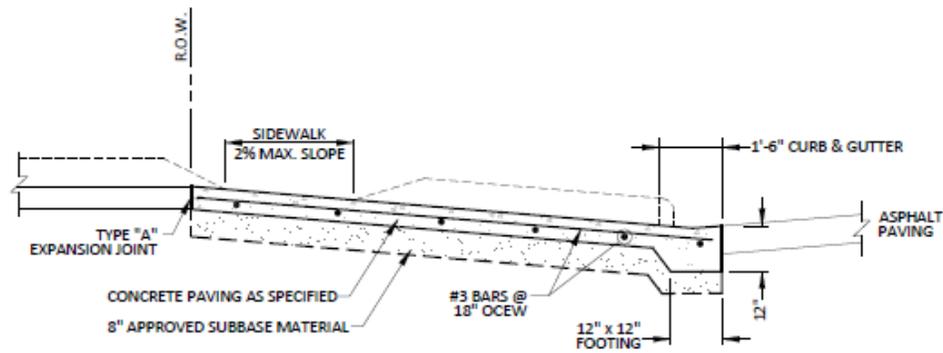


STORAGE LENGTH				
TOTAL PARKING SPACES PROVIDED	MF/COMMERICAL LAND USE		INDUSTRIAL LAND USE	
	AT NON-MEDIAN OPENING (FT)			
LESS THAN 25	15	15	15	33
26 to 50	15	33	15	33
51 to 100	33	33	33	33
101 to 200	33	75	33	55
MORE THAN 200	75	75	55	75

- NOTES:
1. FOR CONSTRUCTION JOINT DETAILS, SEE P-11. FOR EXPANSION JOINT DETAILS, SEE P-12.
 2. STORAGE LENGTHS SHALL BE AS SHOWN OR IN ACCORDANCE WITH THE LATEST VERSION OF CHAPTER 53 OF THE CARROLLTON CODE OF ORDINANCES.
 3. SIDEWALK SHALL BE AT A 2% CROSS SLOPE ACROSS THE DRIVEWAY.



COMMERCIAL APPROACH (CONCRETE PAVING)



COMMERCIAL APPROACH (ASPHALT PAVING)

NOTE:

SEE P-18 FOR BARRIER FREE RAMP DETAIL.

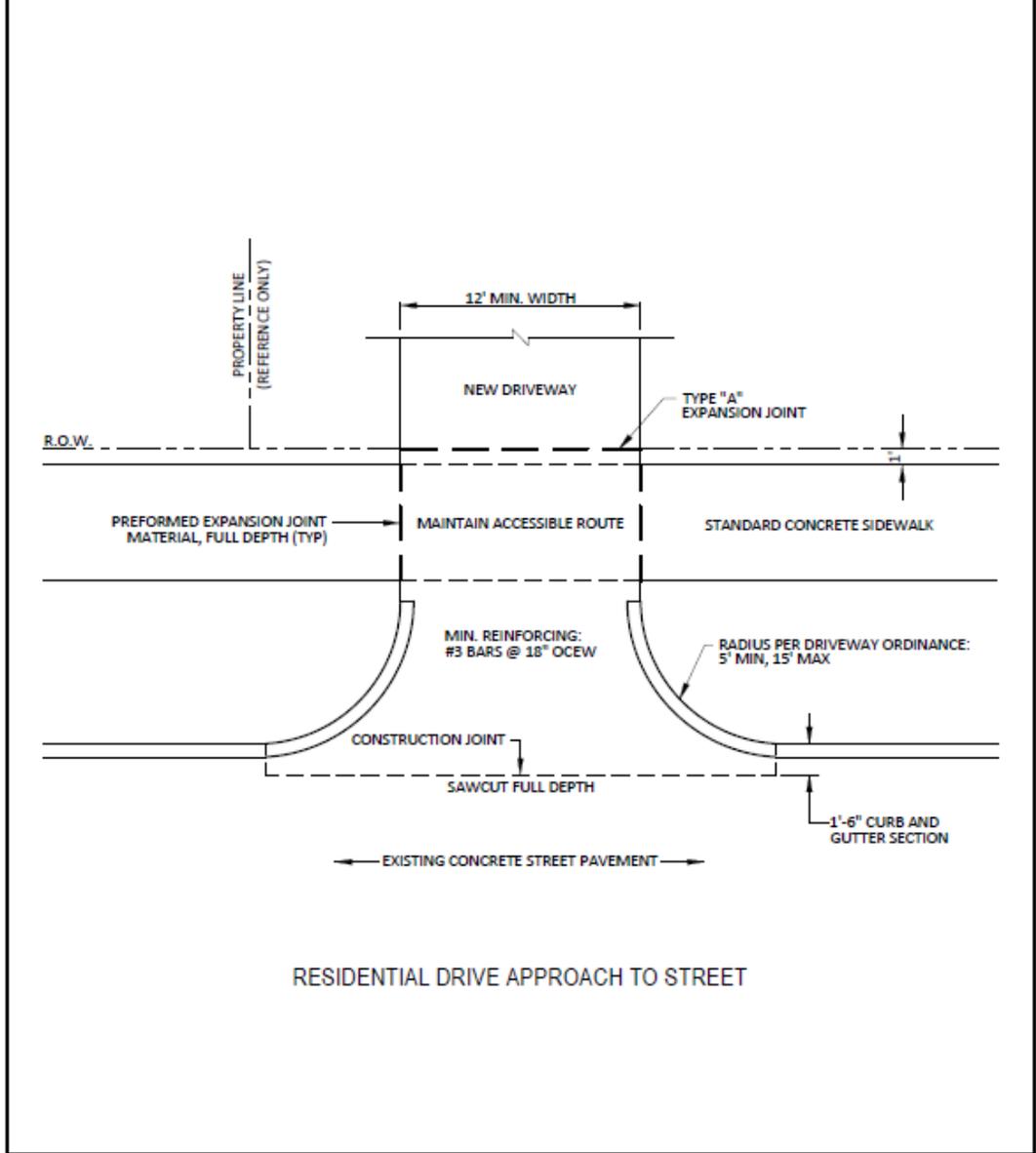


GENERAL DESIGN STANDARDS
PAVING DETAILS

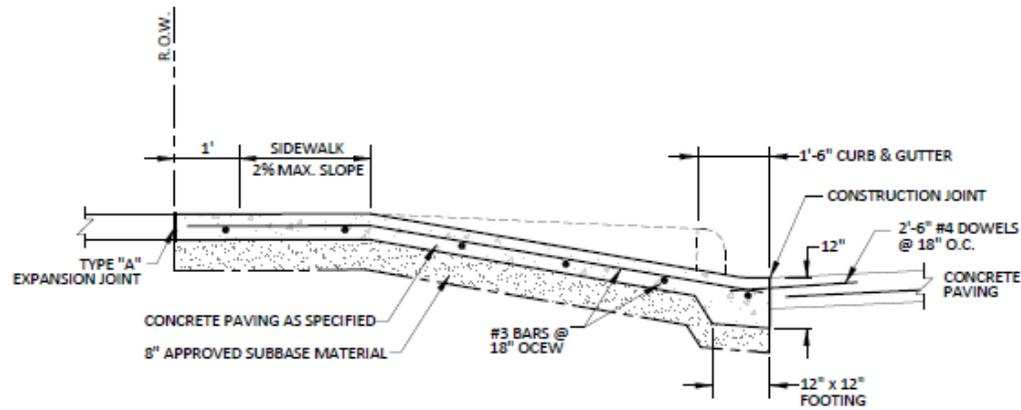
SCALE: NTS DATE: 05/2017
SHEET 2 OF 2

COMMERCIAL DRIVE APPROACH
CROSS SECTIONS

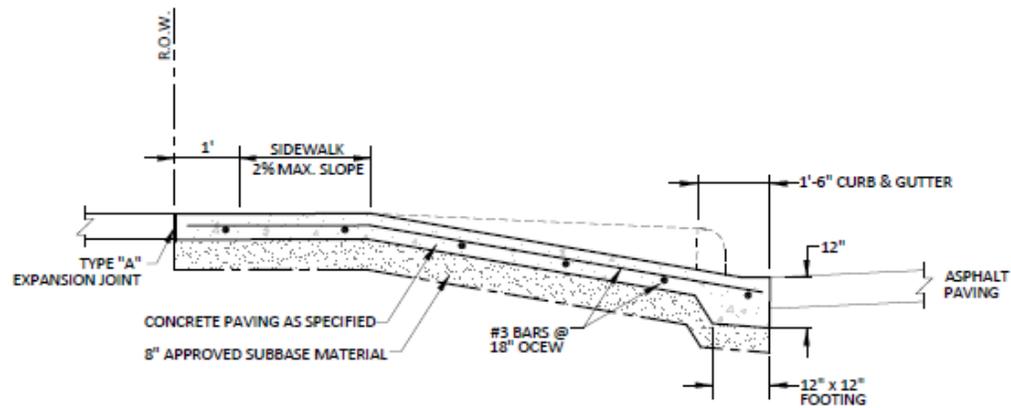
P-15
ENGINEERING
DEPARTMENT



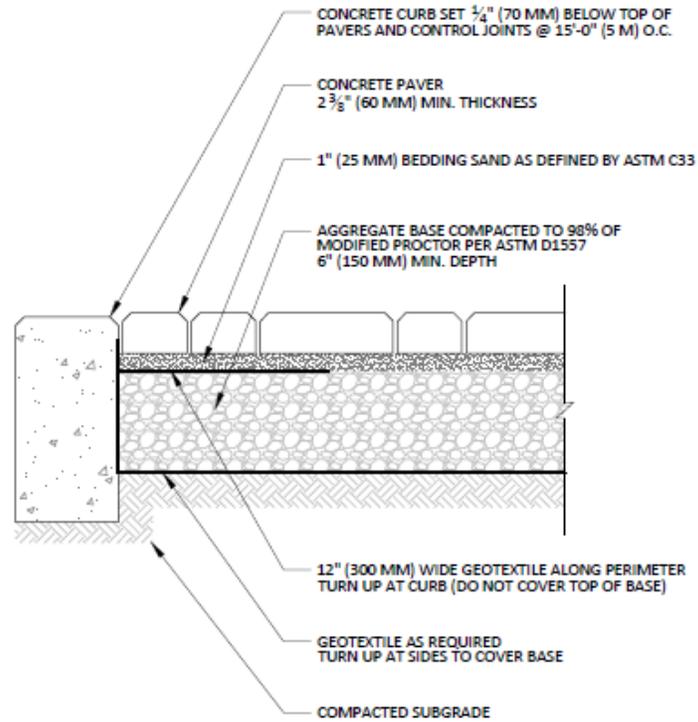
	GENERAL DESIGN STANDARDS PAVING DETAILS	SCALE: NTS DATE: 01/2005 SHEET 1 OF 3
	RESIDENTIAL DRIVE APPROACH PLAN VIEW	P-16 ENGINEERING DEPARTMENT



RESIDENTIAL APPROACH (CONCRETE PAVING)



RESIDENTIAL APPROACH (ASPHALT PAVING)

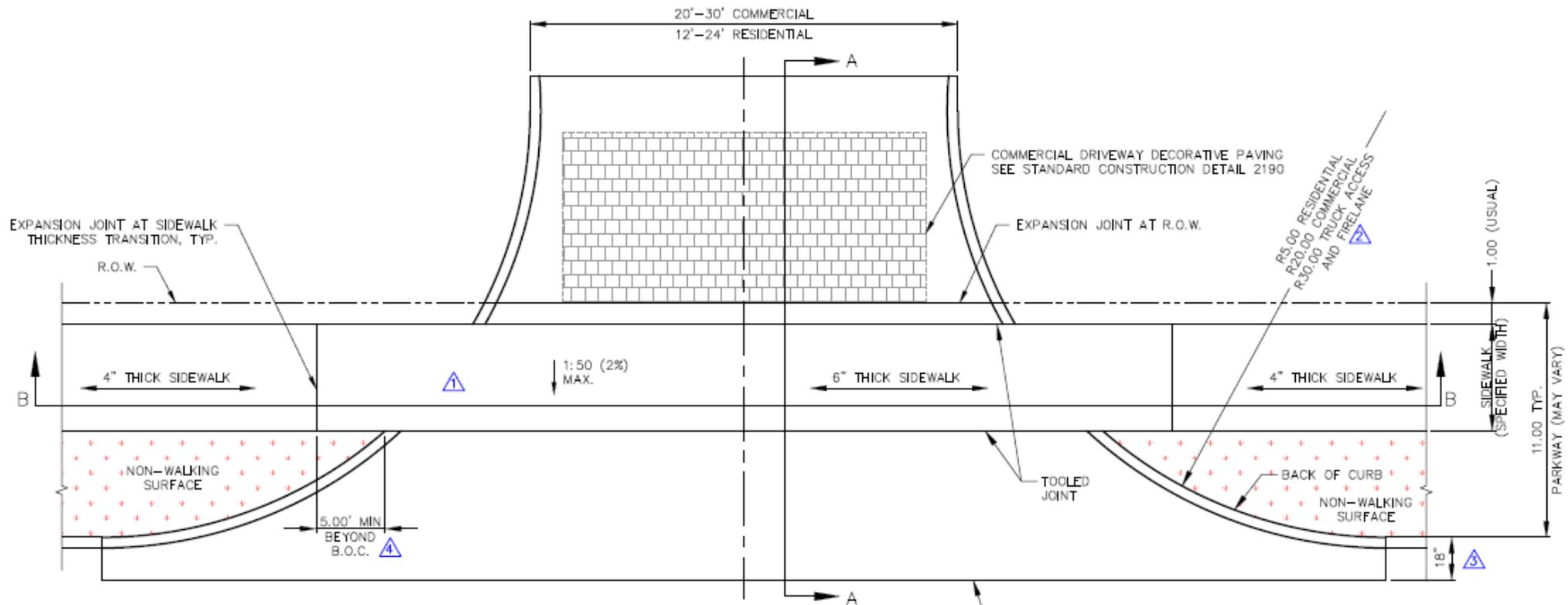


RESIDENTIAL DRIVEWAY WITH CONCRETE PAVERS

NOTES:

1. THICKNESS OF AGGREGATE BASE WILL VARY WITH SUBGRADE CONDITIONS.
2. CONCRETE PAVERS SHOULD BE PLACED ON A CEMENT-TREATED BASE IF SOIL IS EXTREMELY WEAK OR CONSTANTLY SATURATED.
3. BASE MATERIAL SHALL CONFORM TO ASTM D1557.
4. PRECAST CONCRETE EDGING MAY BE USED.

Coppell's Driveway Approaches



NOTES:

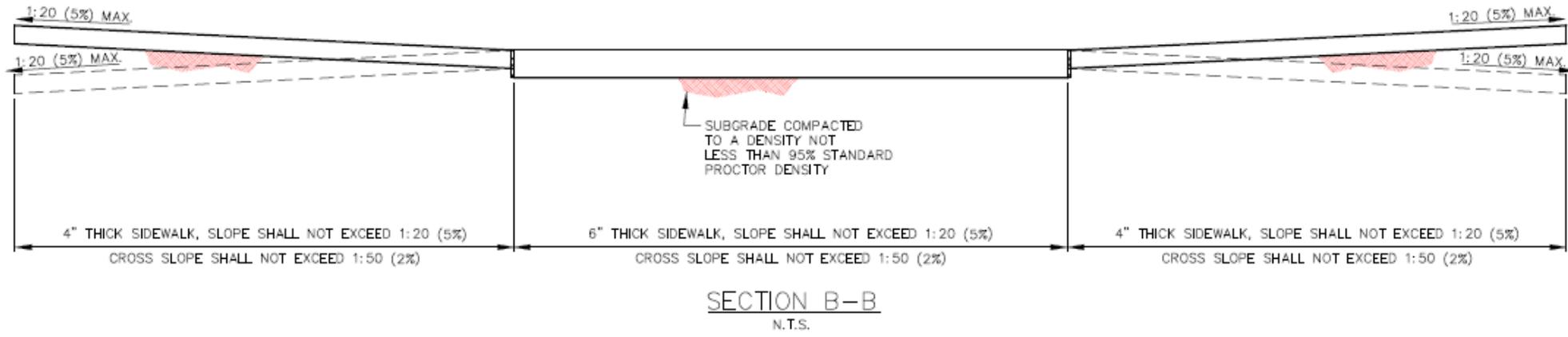
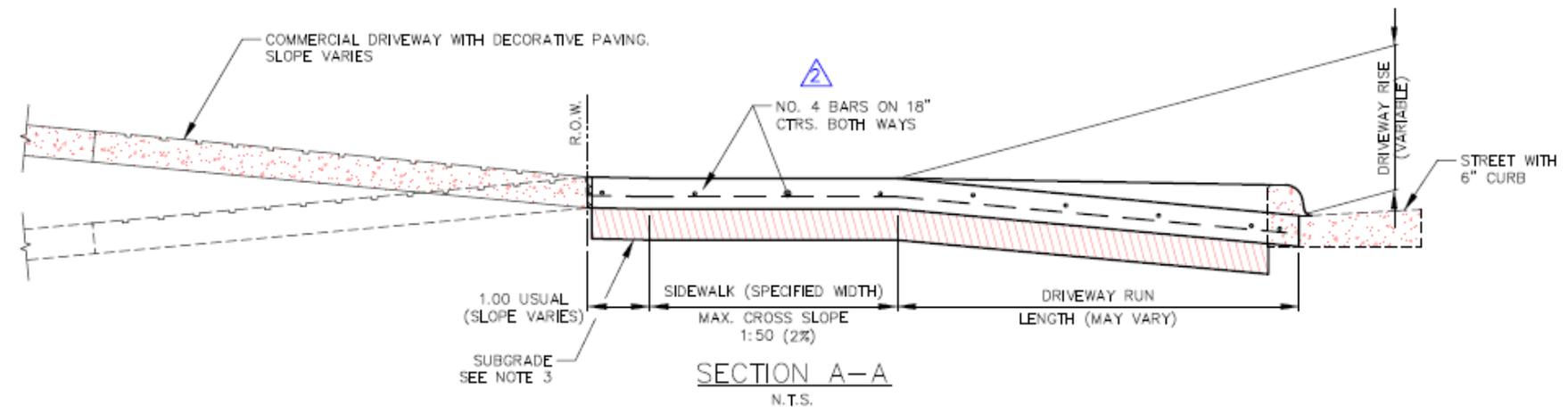
1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 6" - CLASS "C", OR AS SPECIFIED BY CITY.
2. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY CITY. SEE STANDARD CONSTRUCTION DETAIL 2120.
3. SUBGRADE (MIN. REQUIREMENTS—UNLESS OTHERWISE APPROVED BY CITY ENGINEER). SUBGRADE UNDER ALL PAVEMENT SHALL BE INITIALLY MIXED 8" THICK AND RE-MIXED 6" THICK AND SHALL BE STABILIZED WITH 6% MIN. BY WEIGHT OF HYDRATED LIME (GENERALLY ±40 LBS./S.Y.) AND COMPACTED TO A DENSITY NOT LESS THAN 95% STANDARD PROCTOR DENSITY. ALTERNATIVE SUBGRADES, SUPPORTED BY LABORATORY TESTS, MAY BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL.
4. PAVEMENT SURFACE FINISH SHALL BE TRANSVERSE TO TRAFFIC LANES AND SHALL BE BAKER BRUSH FINISH UNLESS OTHERWISE SPECIFIED BY CITY.
5. ALL DIMENSIONS TO BACK OF CURB AND IN FEET UNLESS OTHERWISE SHOWN.
6. WIDTHS AND SLOPES MAY VARY. ALL ACCESSIBLE ROUTES MUST COMPLY WITH A.D.A. AND T.A.S. REGULATIONS.

North Central Texas Council of Governments



NOTE: STANDARDS ARE ADOPTED FROM THE NCTCOG STANDARD DRAWINGS DATED NOV. '96, WITH LOCAL EXCEPTIONS.

NO.	LOCAL EXCEPTION	BY	DATE		STANDARD CONSTRUCTION DETAILS DRIVEWAY APPROACH TYPE "A" CITY OF COPPELL DALLAS COUNTY, TEXAS	STD. SPEC. REFERENCE
▲	ADD DIMENSION.	SWL	JUL '14			301., 303., 305.
▲	CHANGED DIMENSION FROM 2.00' TO 18"	SWL	JUL '14			STANDARD DETAIL
▲	ADDED 30.00' RADIUS FOR TRUCK TRAFFIC AND FIRELANE	SWL	JUL '14			2150-1
▲	DELETED DETECTABLE WARNING AND CALLOUT	SWL	FEB. '19			



NOTES:

1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 6" - CLASS "C", OR AS SPECIFIED BY CITY.
2. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY CITY. SEE STANDARD CONSTRUCTION DETAIL 2120.
3. SUBGRADE (MIN. REQUIREMENTS-UNLESS OTHERWISE APPROVED BY CITY ENGINEER). SUBGRADE UNDER ALL PAVEMENT SHALL BE INITIALLY MIXED 8" THICK AND RE-MIXED 6" THICK AND SHALL BE STABILIZED WITH 5% MIN. BY WEIGHT OF HYDRATED LIME (GENERALLY ±40 LBS./S.Y.) AND COMPACTED TO A DENSITY NOT LESS THAN 95% STANDARD PROCTOR DENSITY. ALTERNATIVE SUBGRADES, SUPPORTED BY LABORATORY TESTS, MAY BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL.

North Central Texas Council of Governments



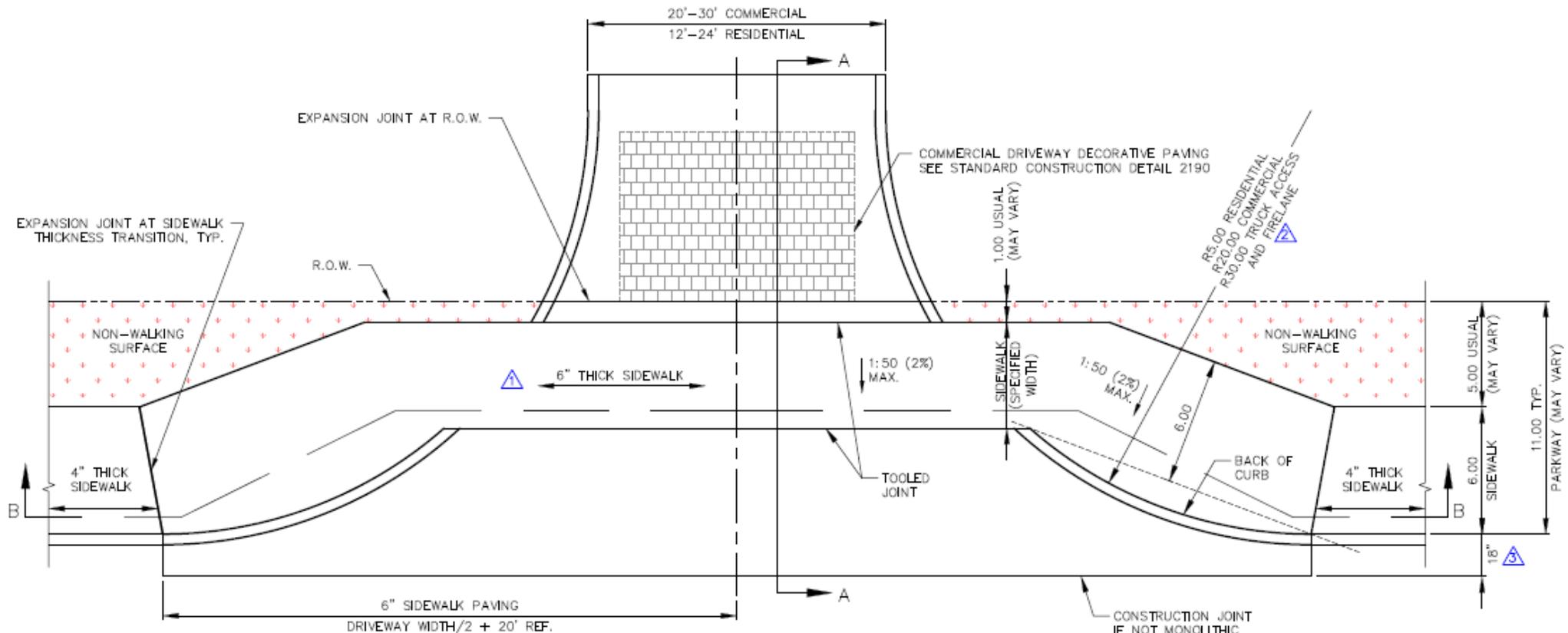
NOTE: STANDARDS ARE ADOPTED FROM THE NCTCOG STANDARD DRAWINGS DATED NOV. '96, WITH LOCAL EXCEPTIONS.

NO.	LOCAL EXCEPTION	BY	DATE
	CHANGED BAR SIZE FROM NO. 3 TO NO. 4	SWL	FEB. '19
	ADDED NOTES	SWL	JUL. '14



STANDARD CONSTRUCTION DETAILS
 DRIVEWAY APPROACH
 TYPE "A"
 CITY OF COPPELL, DALLAS COUNTY, TEXAS

STD. SPEC. REFERENCE
 301., 303., 305.
 STANDARD DETAIL
 2150-2



NOTES:

1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 6" - CLASS "C", OR AS SPECIFIED BY CITY.
2. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY CITY. SEE STANDARD CONSTRUCTION DETAIL 2120.
3. SUBGRADE (MIN. REQUIREMENTS—UNLESS OTHERWISE APPROVED BY CITY ENGINEER). SUBGRADE UNDER ALL PAVEMENT SHALL BE INITIALLY MIXED 8" THICK AND RE-MIXED 6" THICK AND SHALL BE STABILIZED WITH 6% MIN. BY WEIGHT OF HYDRATED LIME (GENERALLY ±40 LBS./5.Y.) AND COMPACTED TO A DENSITY NOT LESS THAN 95% STANDARD PROCTOR DENSITY. ALTERNATIVE SUBGRADES, SUPPORTED BY LABORATORY TESTS, MAY BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL.
4. PAVEMENT SURFACE FINISH SHALL BE TRANSVERSE TO TRAFFIC LANES AND SHALL BE BAKER BROOM FINISH UNLESS OTHERWISE SPECIFIED BY CITY.
5. ALL DIMENSIONS TO BACK OF CURB AND IN FEET UNLESS OTHERWISE SHOWN.
6. WIDTHS AND SLOPES MAY VARY. ALL ACCESSIBLE ROUTES MUST COMPLY WITH A.D.A. AND T.A.S. REGULATIONS.

North Central Texas Council of Governments



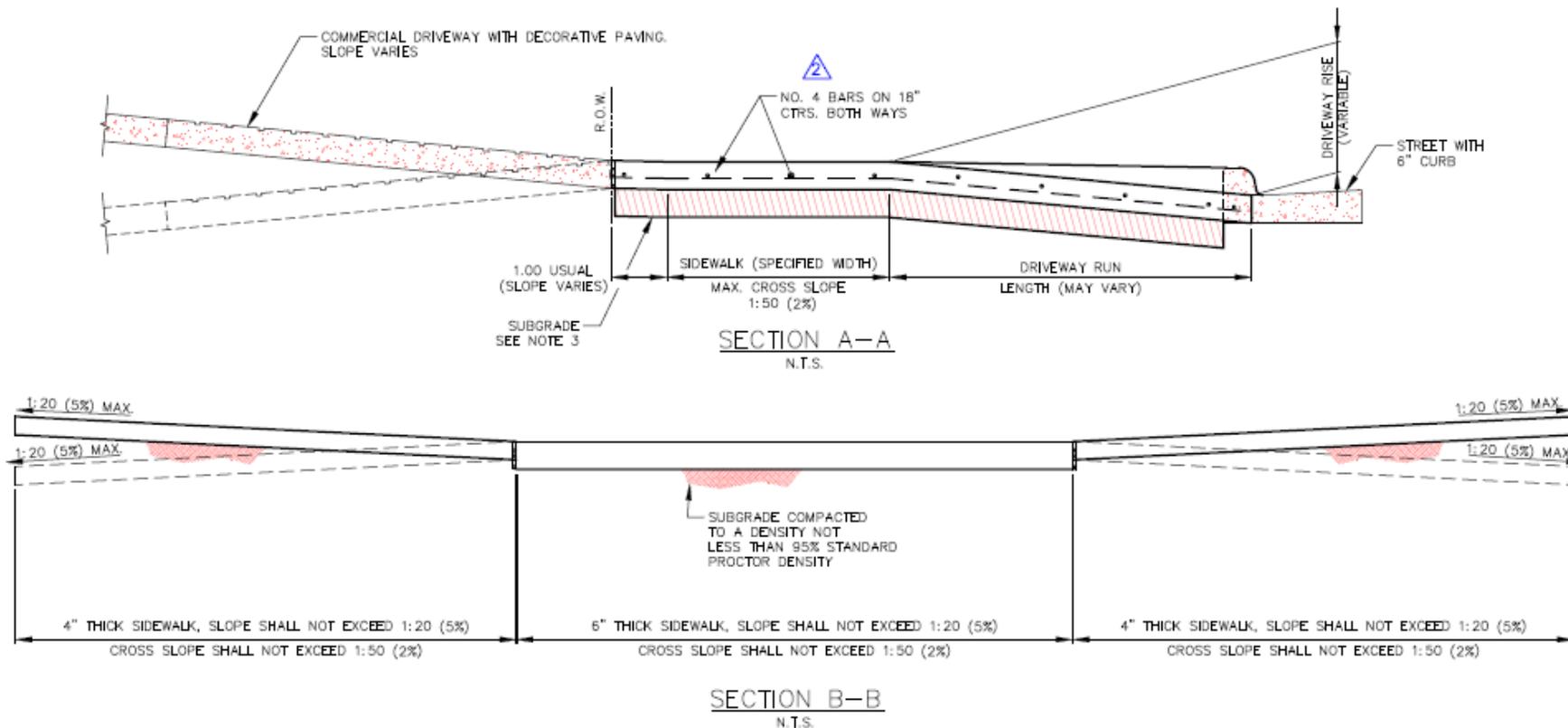
NOTE: STANDARDS ARE ADOPTED FROM THE NCTCG STANDARD DRAWINGS DATED NOV. '96, WITH LOCAL EXCEPTIONS.

NO.	LOCAL EXCEPTION	BY	DATE



STANDARD CONSTRUCTION DETAILS
DRIVEWAY APPROACH
TYPE "B"
 CITY OF COPPELL, DALLAS COUNTY, TEXAS

STD. SPEC. REFERENCE
301., 303., 305.
 STANDARD DETAIL
2155-1



NOTES:

1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 6" - CLASS "C", OR AS SPECIFIED BY CITY.
2. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY CITY. SEE STANDARD CONSTRUCTION DETAIL 2120.
3. SUBGRADE (MIN. REQUIREMENTS—UNLESS OTHERWISE APPROVED BY CITY ENGINEER). SUBGRADE UNDER ALL PAVEMENT SHALL BE INITIALLY MIXED 6" THICK AND RE-MIXED 6" THICK AND SHALL BE STABILIZED WITH 6% MIN. BY WEIGHT OF HYDRATED LIME (GENERALLY ±40 LBS./S.Y.) AND COMPACTED TO A DENSITY NOT LESS THAN 95% STANDARD PROCTOR DENSITY. ALTERNATIVE SUBGRADES, SUPPORTED BY LABORATORY TESTS, MAY BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL.

North Central Texas Council of Governments



NOTE: STANDARDS ARE ADOPTED FROM THE NCTCOG STANDARD DRAWINGS DATED NOV. '96, WITH LOCAL EXCEPTIONS.

NO.	LOCAL EXCEPTION	BY	DATE
	CHANGED BAR SIZE FROM NO. 3 TO NO. 4	SWL	FEB. '19
	ADDED NOTES	SWL	JUL. '14



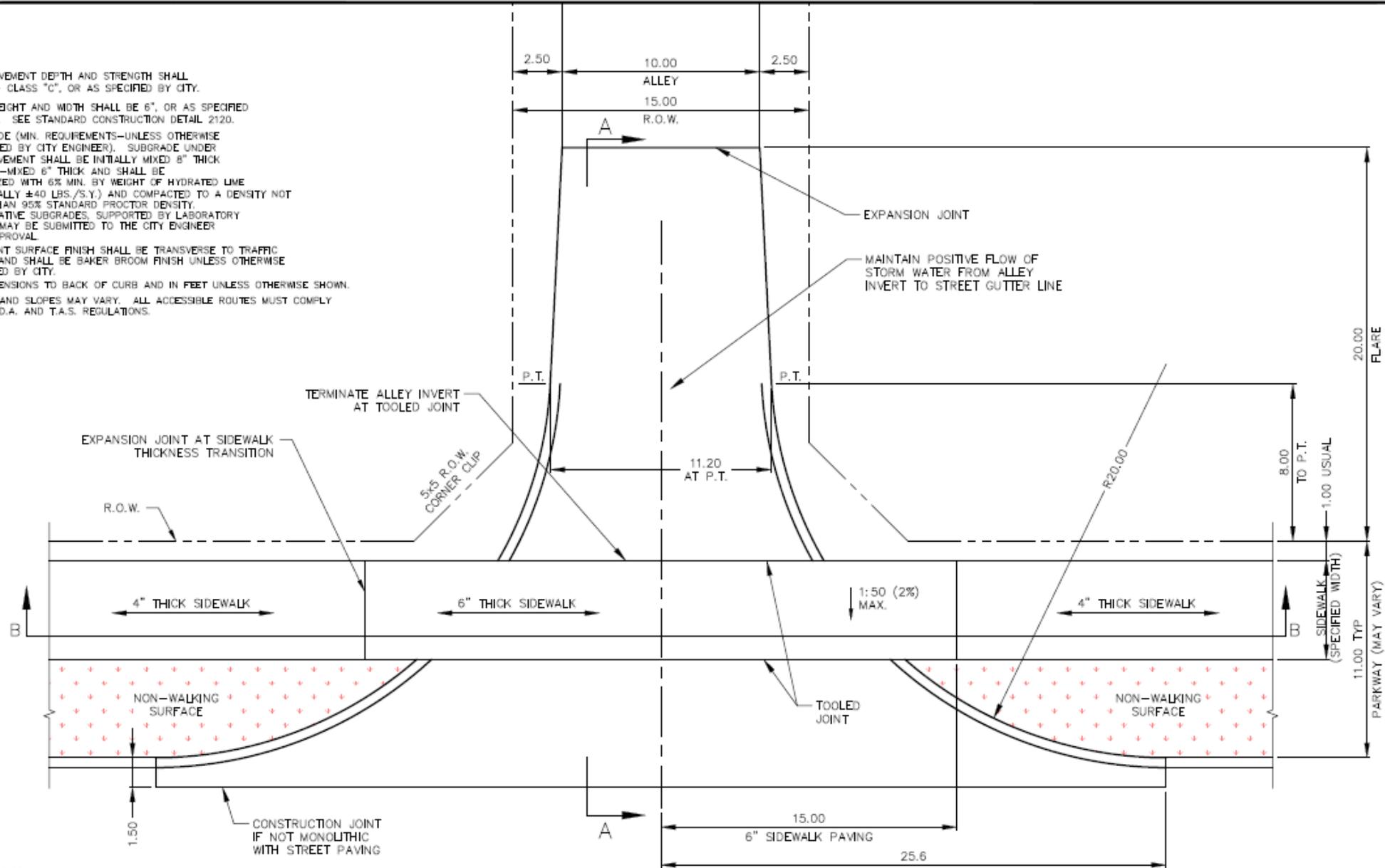
STANDARD CONSTRUCTION DETAILS	
DRIVEWAY APPROACH TYPE "B"	
CITY OF COPPELL, DALLAS COUNTY, TEXAS	

STD. SPEC. REFERENCE 301, 303, 305.
STANDARD DETAIL 2155-2

FILE NO. 2155-2 (REVISED) DATE: 07/14/14

NOTES:

1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 6" - CLASS "C", OR AS SPECIFIED BY CITY.
2. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY CITY. SEE STANDARD CONSTRUCTION DETAIL 2120.
3. SUBGRADE (MIN. REQUIREMENTS—UNLESS OTHERWISE APPROVED BY CITY ENGINEER). SUBGRADE UNDER ALL PAVEMENT SHALL BE INITIALLY MIXED 8" THICK AND RE-MIXED 6" THICK AND SHALL BE STABILIZED WITH 6% MIN. BY WEIGHT OF HYDRATED LIME (GENERALLY ±40 LBS./5.Y.) AND COMPACTED TO A DENSITY NOT LESS THAN 95% STANDARD PROCTOR DENSITY. ALTERNATIVE SUBGRADES, SUPPORTED BY LABORATORY TESTS, MAY BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL.
4. PAVEMENT SURFACE FINISH SHALL BE TRANSVERSE TO TRAFFIC LANES AND SHALL BE BAKER BROOM FINISH UNLESS OTHERWISE SPECIFIED BY CITY.
5. ALL DIMENSIONS TO BACK OF CURB AND IN FEET UNLESS OTHERWISE SHOWN.
6. WIDTHS AND SLOPES MAY VARY. ALL ACCESSIBLE ROUTES MUST COMPLY WITH A.D.A. AND T.A.S. REGULATIONS.



North Central Texas Council of Governments



NOTE: STANDARDS ARE ADOPTED FROM THE NCTCOG STANDARD DRAWINGS DATED NOV. '96, WITH LOCAL EXCEPTIONS.

NO.	LOCAL EXCEPTION	BY	DATE

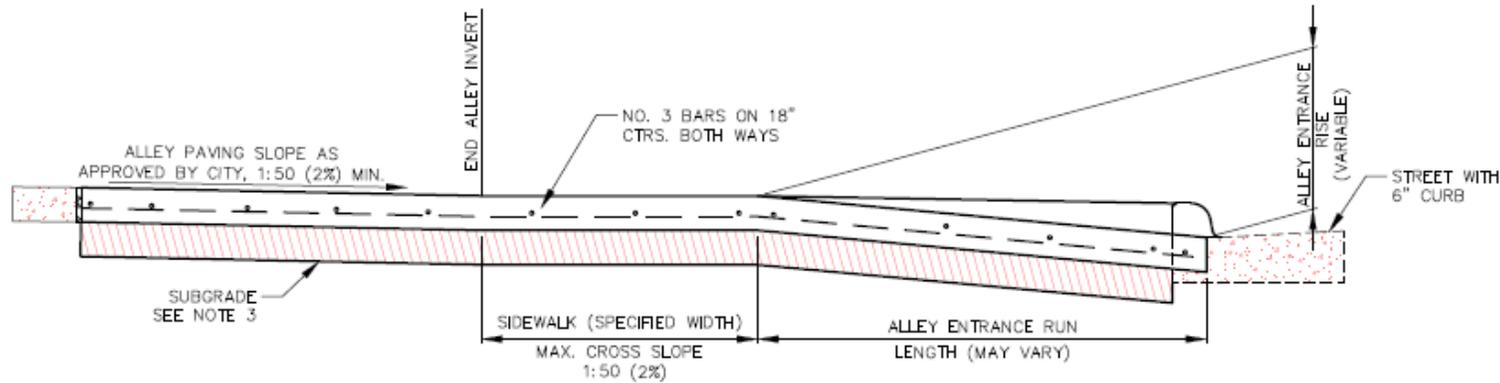


STANDARD CONSTRUCTION DETAILS

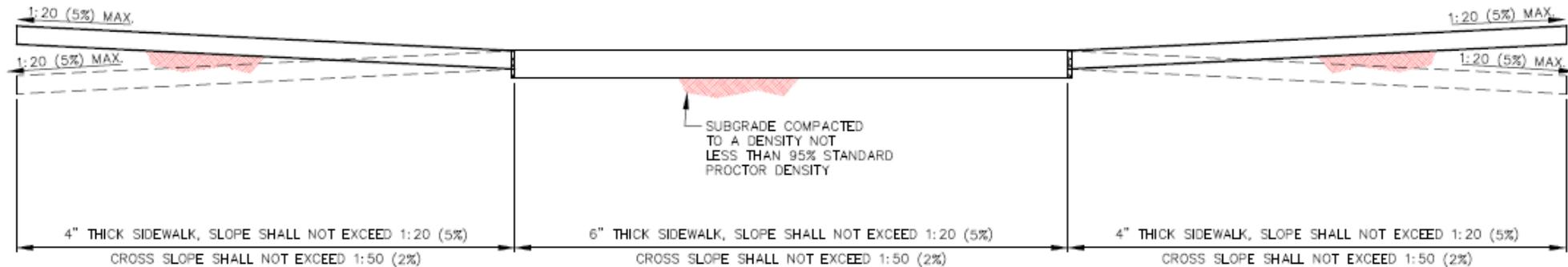
ALLEY APPROACH
RADIUS RETURN TYPE

CITY OF COPPELL DALLAS COUNTY, TEXAS

STD. SPEC. REFERENCE
301., 303., 305.
STANDARD DETAIL
2160-1



SECTION A-A
N.T.S.



SECTION B-B
N.T.S.

NOTES:

1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 6" - CLASS "C", OR AS SPECIFIED BY CITY.
2. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY CITY. SEE STANDARD CONSTRUCTION DETAIL 2120.
3. SUBGRADE (MIN. REQUIREMENTS—UNLESS OTHERWISE APPROVED BY CITY ENGINEER). SUBGRADE UNDER ALL PAVEMENT SHALL BE INITIALLY MIXED 8" THICK AND RE-MIXED 6" THICK AND SHALL BE STABILIZED WITH 6% MIN. BY WEIGHT OF HYDRATED LIME (GENERALLY ±40 LBS./S.Y.) AND COMPACTED TO A DENSITY NOT LESS THAN 95% STANDARD PROCTOR DENSITY. ALTERNATIVE SUBGRADES, SUPPORTED BY LABORATORY TESTS, MAY BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL.

North Central Texas Council of Governments



NOTE: STANDARDS ARE ADOPTED FROM THE NCTCG STANDARD DRAWINGS DATED NOV. '96, WITH LOCAL EXCEPTIONS.

	ADDED NOTES	SWL	JUL '14
NO.	LOCAL EXCEPTION	BY	DATE



STANDARD CONSTRUCTION DETAILS

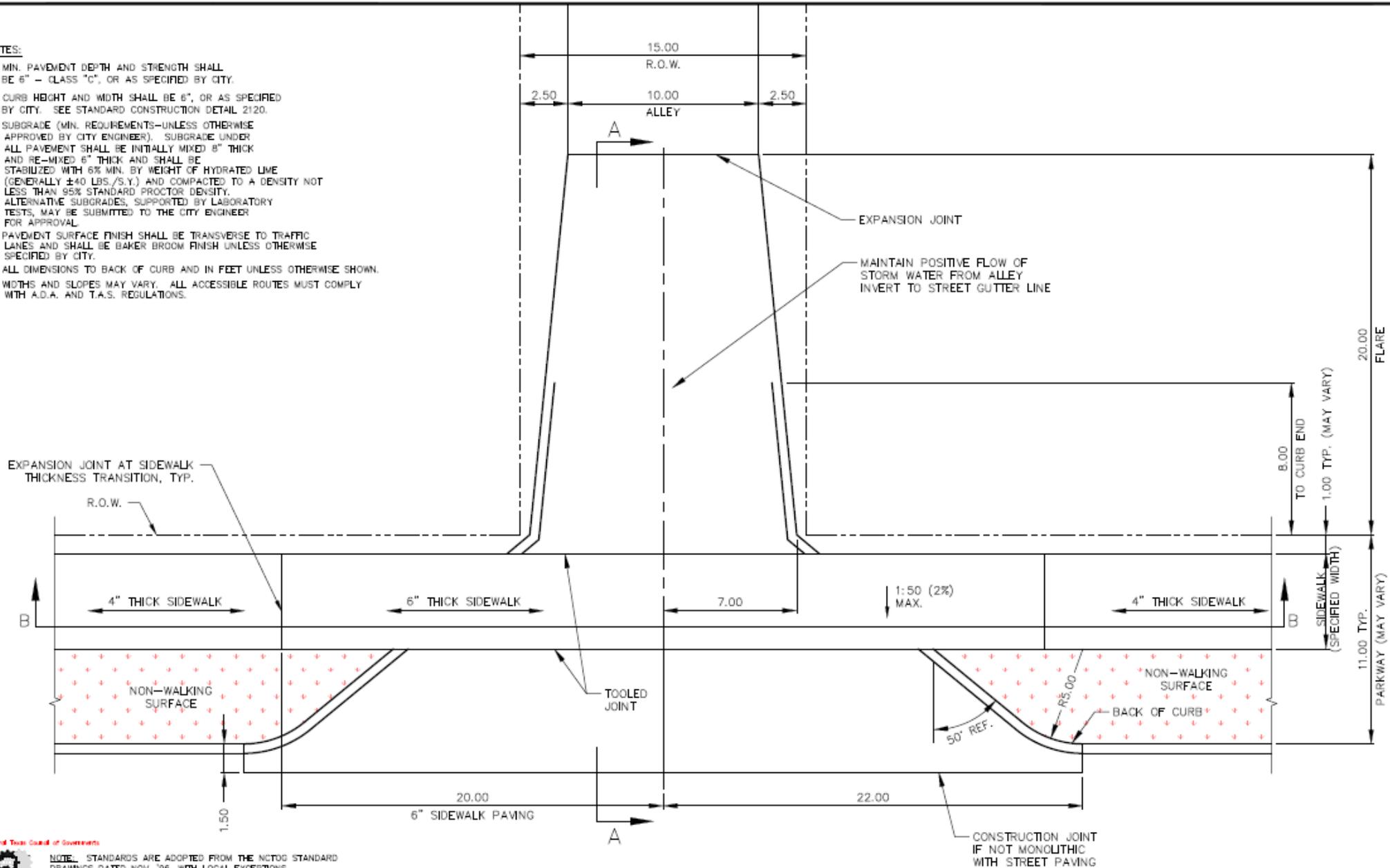
ALLEY APPROACH
RADIUS RETURN TYPE

CITY OF COPPELL DALLAS COUNTY, TEXAS

STD. SPEC. REFERENCE
301., 303., 305.
STANDARD DETAIL
2160-2

NOTES:

1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 6" - CLASS "C", OR AS SPECIFIED BY CITY.
2. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY CITY. SEE STANDARD CONSTRUCTION DETAIL 2120.
3. SUBGRADE (MIN. REQUIREMENTS—UNLESS OTHERWISE APPROVED BY CITY ENGINEER). SUBGRADE UNDER ALL PAVEMENT SHALL BE INITIALLY MIXED 8" THICK AND RE-MIXED 6" THICK AND SHALL BE STABILIZED WITH 6% MIN. BY WEIGHT OF HYDRATED LIME (GENERALLY ±40 LBS./S.Y.) AND COMPACTED TO A DENSITY NOT LESS THAN 95% STANDARD PROCTOR DENSITY. ALTERNATIVE SUBGRADES, SUPPORTED BY LABORATORY TESTS, MAY BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL.
4. PAVEMENT SURFACE FINISH SHALL BE TRANSVERSE TO TRAFFIC LANES AND SHALL BE BAKER BROOM FINISH UNLESS OTHERWISE SPECIFIED BY CITY.
5. ALL DIMENSIONS TO BACK OF CURB AND IN FEET UNLESS OTHERWISE SHOWN.
6. WIDTHS AND SLOPES MAY VARY. ALL ACCESSIBLE ROUTES MUST COMPLY WITH A.D.A. AND T.A.S. REGULATIONS.



North Central Texas Council of Governments



NOTE: STANDARDS ARE ADOPTED FROM THE NCTCG STANDARD DRAWINGS DATED NOV. '96, WITH LOCAL EXCEPTIONS.

NO.	LOCAL EXCEPTION	BY	DATE

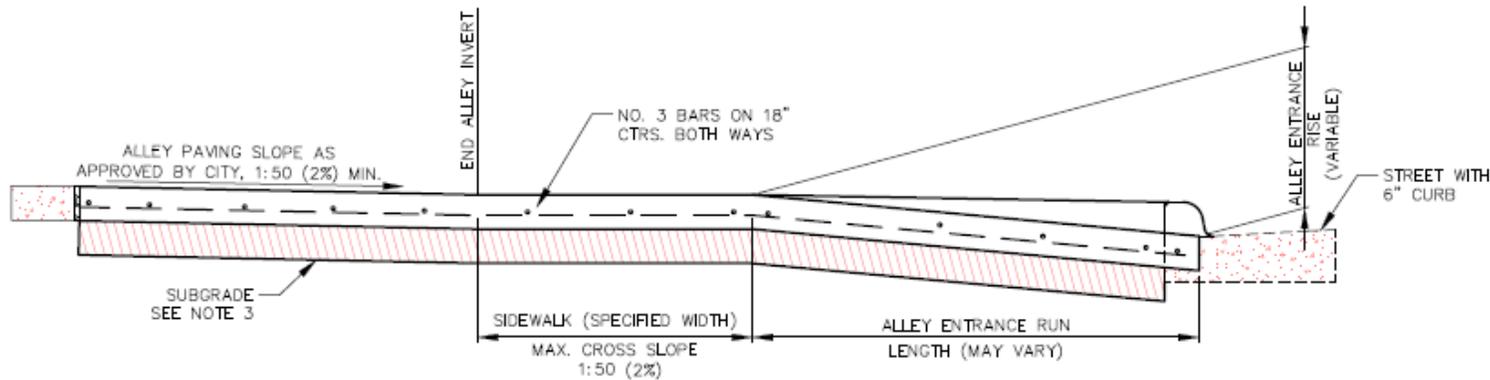


STANDARD CONSTRUCTION DETAILS

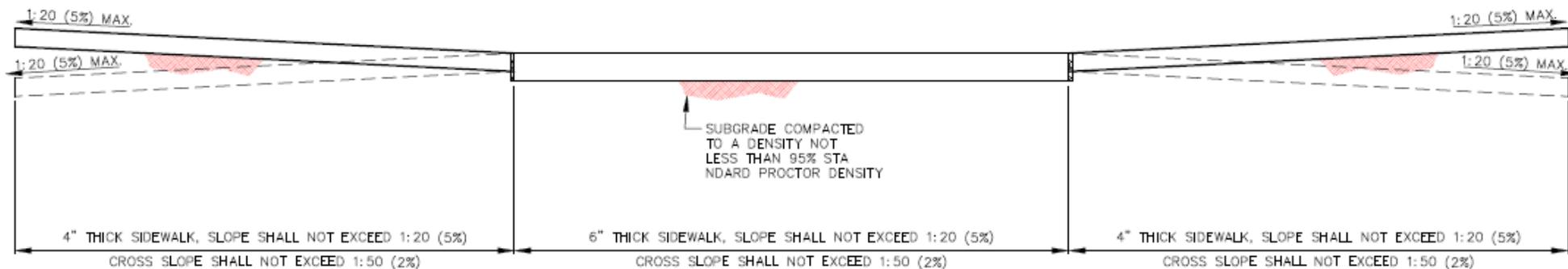
ALLEY APPROACH REPLACEMENT
FLARED TYPE

CITY OF COPPELL DALLAS COUNTY, TEXAS

STD. SPEC. REFERENCE
301., 303., 305.
STANDARD DETAIL
2165-1



SECTION A-A
N.T.S.



SECTION B-B
N.T.S.

NOTES:

1. MIN. PAVEMENT DEPTH AND STRENGTH SHALL BE 6" - CLASS "C", OR AS SPECIFIED BY CITY.
2. CURB HEIGHT AND WIDTH SHALL BE 6", OR AS SPECIFIED BY CITY. SEE STANDARD CONSTRUCTION DETAIL 2120.
3. SUBGRADE (MIN. REQUIREMENTS—UNLESS OTHERWISE APPROVED BY CITY ENGINEER). SUBGRADE UNDER ALL PAVEMENT SHALL BE INITIALLY MIXED 8" THICK AND RE-MIXED 6" THICK AND SHALL BE STABILIZED WITH 6% MIN. BY WEIGHT OF HYDRATED LIME (GENERALLY ±40 LBS./S.Y.) AND COMPACTED TO A DENSITY NOT LESS THAN 95% STANDARD PROCTOR DENSITY. ALTERNATIVE SUBGRADES, SUPPORTED BY LABORATORY TESTS, MAY BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL.

North Central Texas Council of Governments



NOTE: STANDARDS ARE ADOPTED FROM THE NCTCOG STANDARD DRAWINGS DATED NOV. '96, WITH LOCAL EXCEPTIONS.

	ADDED NOTES	SWL	JUL '14
NO.	LOCAL EXCEPTION	BY	DATE



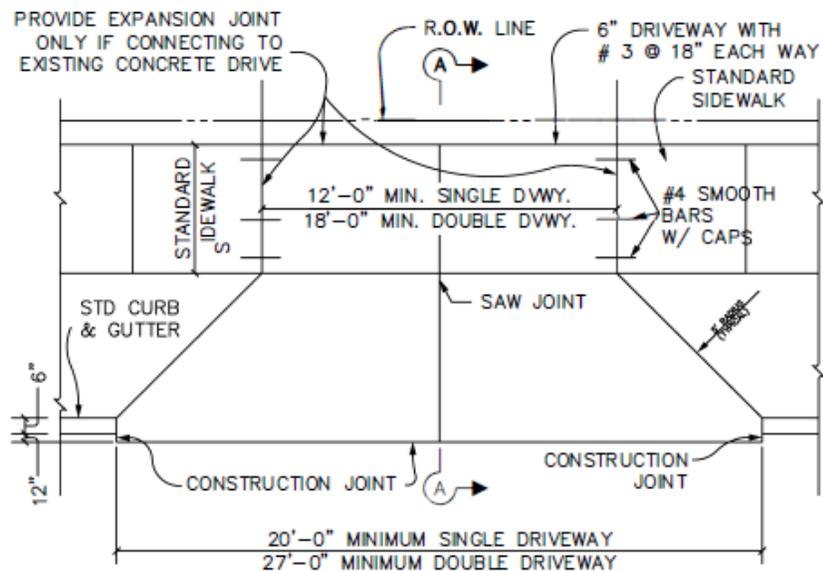
STANDARD CONSTRUCTION DETAILS

ALLEY APPROACH REPLACEMENT
FLARED TYPE

CITY OF COPPELL DALLAS COUNTY, TEXAS

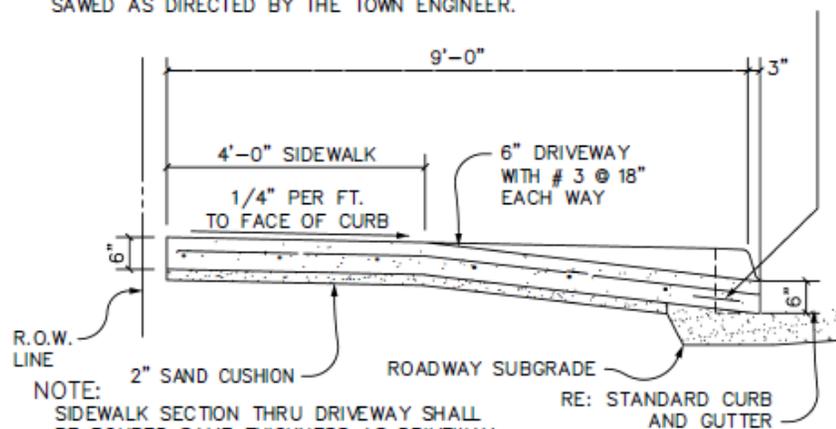
STD. SPEC.
REFERENCE
301., 303., 305.
STANDARD DETAIL
2165-2

Flower Mound's Driveway Approaches



NOTE:
EXISTING CURB AND GUTTER, IF ANY, MUST BE SAWED AS DIRECTED BY THE TOWN ENGINEER.

8" PENETRATION EPOXY
#4 DEFORMED BAR



NOTE:
SIDEWALK SECTION THRU DRIVEWAY SHALL BE POURED SAME THICKNESS AS DRIVEWAY APPROACH (EXISTING SIDEWALK, IF ANY, SHALL BE REMOVED AND REPLACED).

SECTION A-A

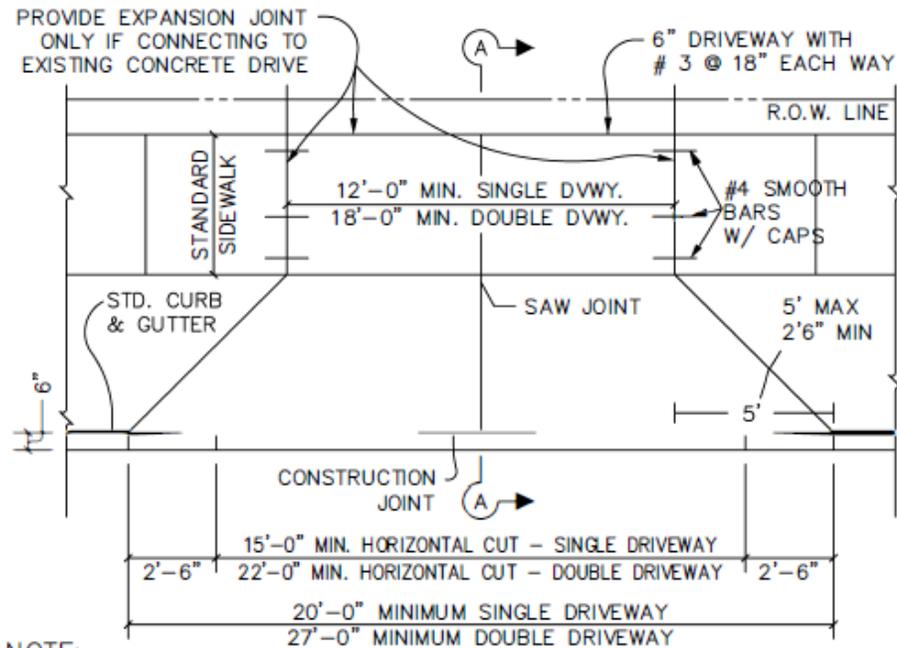


STANDARD PAVING DETAILS
DRIVEWAY APPROACH/HORIZONTAL CURB CUT
ON EXISTING ROADWAY

DATE: OCT 2009

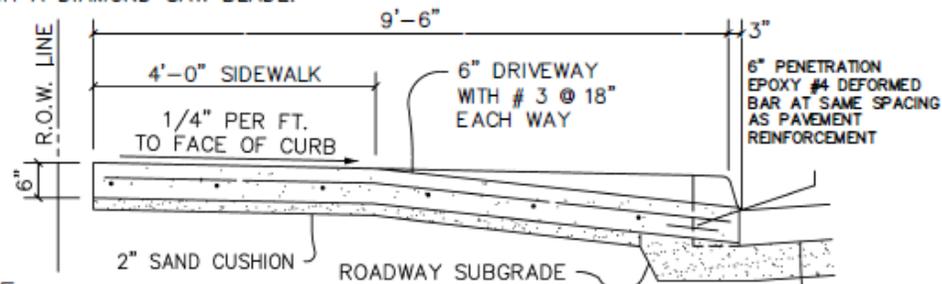
SHEET 3 OF 9

SHEET: ST-8



NOTE:

EXISTING CURB AND GUTTER, IF ANY, MUST BE SAWED AS DIRECTED BY THE TOWN ENGINEER. HORIZONTAL CURB CUT SHALL BE MADE AT AN ELEVATION OF 1/2" ABOVE THE EXISTING GUTTER WITH A MINIMUM LENGTH AS SHOWN. THE TRANSITIONAL SAW CUT SHALL HAVE A RUN OF 2'-6" AND SHALL RISE TO MEET THE EXISTING TOP OF CURB. ALL EXPOSED EDGES SHALL BE GROUND TO A 1/4" RADIUS. SAW CUTTING SHALL BE PERFORMED WITH A RIDE-ON SAW EQUIPPED WITH A DIAMOND SAW BLADE.



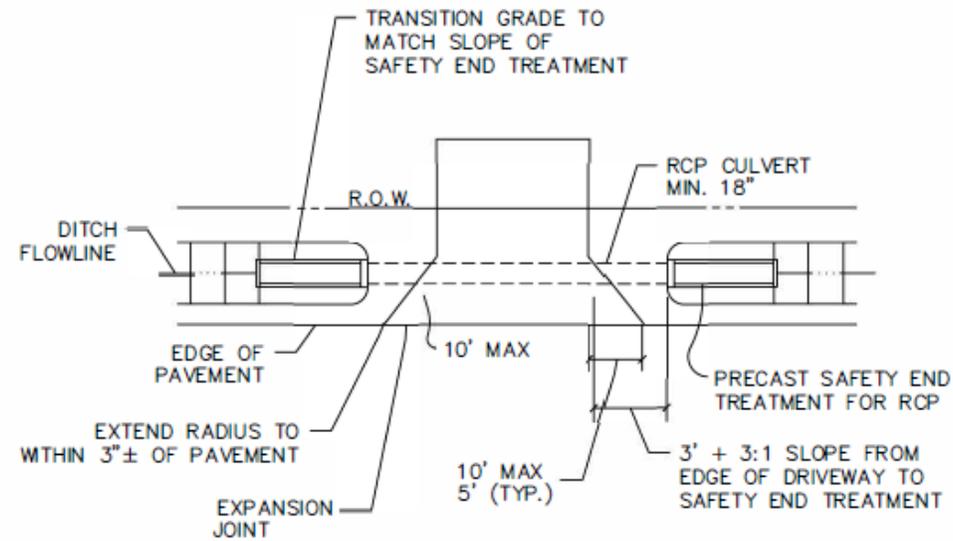
NOTE:

SIDEWALK SECTION THRU DRIVEWAY SHALL BE POURED SAME THICKNESS AS DRIVEWAY APPROACH (EXISTING SIDEWALK, IF ANY, SHALL BE REMOVED AND REPLACED).

RE: STANDARD CURB AND GUTTER

SECTION A-A

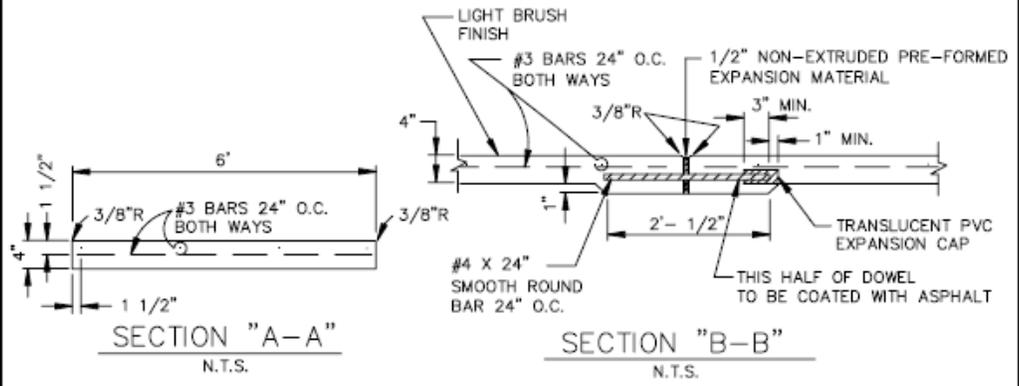
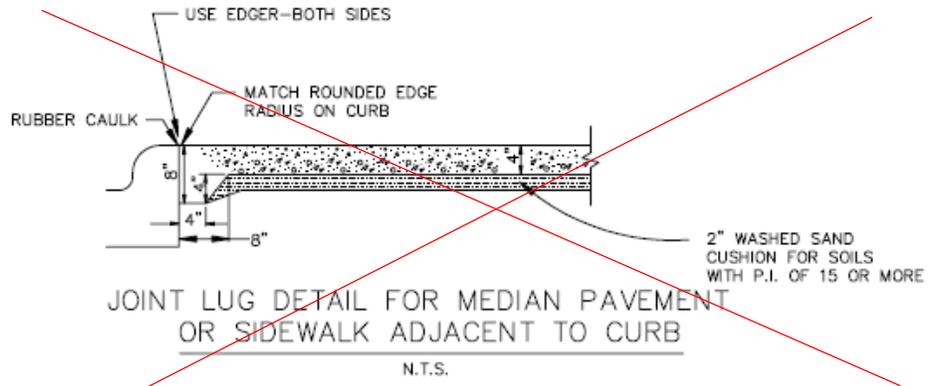
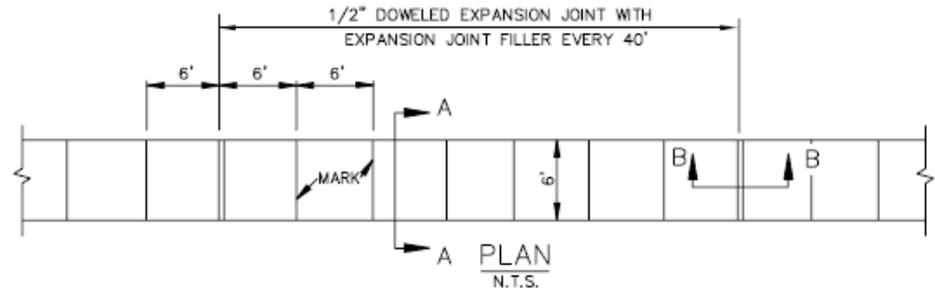




NOTES:

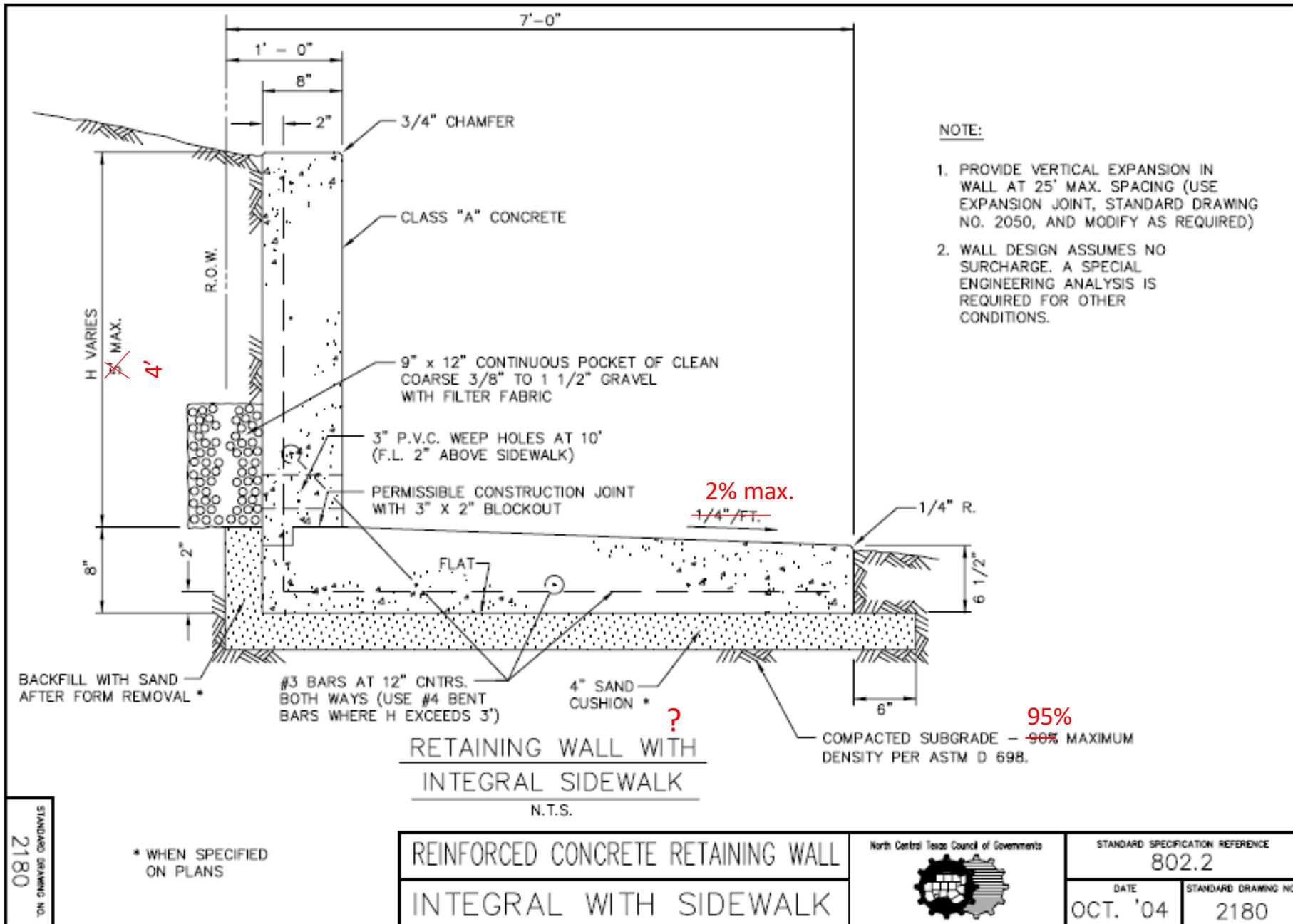
1. PROVIDE EXPANSION JOINT IF DRIVEWAY APPROACH CONNECTS WITH EXISTING CONCRETE DRIVEWAY.
2. DRIVEWAY APPROACH SHALL BE 6" REINFORCED CONCRETE PAVEMENT WITH #3 @ 18" EACH WAY.
3. MINIMUM WIDTH FOR SINGLE DRIVEWAYS IS 12'-0" AT R.O.W. MINIMUM WIDTH FOR DOUBLE DRIVEWAYS IS 18'-0" AT R.O.W.
4. INSTALL PRECAST SAFETY END TREATMENTS FOR RCP PER TXDOT STANDARDS OR AS APPROVED BY TOWN ENGINEER.
5. MINIMUM CULVERT SIZE IS 18".





- NOTE:
1. REFER TO STANDARD SPECIFICATION ITEM 305.2 FOR ALTERNATE REINFORCEMENT.
 2. CROSS SLOPE OF SIDEWALK SHALL BE ~~1/4" PER FT. MIN. TO 3/8" PER FT. MAX.~~ **2% max.**
 3. OTHER THAN 6'-0" SIDEWALK WIDTH MAY BE SPECIFIED BY OWNER. **Class C**
 4. SIDEWALK SHALL BE CLASS "A" CONCRETE UNLESS OTHERWISE SPECIFIED BY OWNER.
 5. ALL HONEYCOMB IN BACK OF CURB TO BE TROWEL-PLASTERED BEFORE POURING SIDEWALK.
 6. LUG MAY BE FORMED BY SHAPING SUBGRADE TO APPROXIMATE DIMENSIONS SHOWN.

REINFORCED CONCRETE SIDEWALKS	North Central Texas Council of Governments	STANDARD SPECIFICATION REFERENCE	
		305.2	
JOINTS AND SPACING		DATE	STANDARD DRAWING NO.
		OCT. '04	2170



GENERAL NOTES:

1. REINFORCED CONCRETE PAVEMENT:
 - A. ALL CURBS SHALL BE PLACED INTEGRAL WITH PAVEMENT UNLESS OTHERWISE APPROVED BY THE OWNER.
 - B. CURBS SHALL MEET THE SAME COMPRESSIVE STRENGTH AS SPECIFIED FOR THE PAVEMENT.
 - C. BAR LAPS SHALL BE 30 DIAMETERS.
 - D. REINFORCING BARS SHALL BE SUPPORTED BY CHAIRS OR OTHER DEVICES APPROVED BY THE OWNER.

2. SUBGRADE: (UNLESS OTHERWISE SPECIFIED BY OWNER)
 - A. SUBGRADE UNDER ALL PAVEMENTS SHALL BE STABILIZED TO A MINIMUM DEPTH OF 6" WITH HYDRATED LIME OR CEMENT WHEN THE P.I. OF THE INPLACE MATERIAL IS GREATER THAN 15. LABORATORY TESTS MUST BE PERFORMED TO DETERMINE THE AMOUNT OF LIME OR CEMENT REQUIRED TO LOWER THE P.I. TO 15 OR BELOW. SATURATION P.I. (PH \geq 12.4) WILL BE THE LIMIT WHEN A SOIL'S P.I. CANNOT BE BROUGHT TO 15 OR LOWER.

 - B. WHERE THE INPLACE MATERIAL HAS A P.I. OF LESS THAN 15, THE SUBGRADE SHALL BE SCARIFIED TO A MINIMUM DEPTH OF 6" AND RECOMPACTED.

3. IF THE ROADWAY IS A DESIGNATED BIKE ROUTE OR BIKE USAGE IS ANTICIPATED, REFER TO NCTCOG'S REGIONAL BICYCLE AND PEDESTRIAN FACILITIES DESIGN MANUAL FOR DESIGN GUIDANCE.

C. Where sulfates are present, consult a geotechnical engineer for recommended subgrade treatment

If the PI is 15 or greater lime shall be used, if the PI is less than 15 cement shall be used or as recommended by a geotech engineer

American Association of State Highway and Transportation Officials **AASHTO Guide for the Development of Bicycle Facilities (2012, 4th Edition)** or the Texas Manual on Uniform Traffic Control Devices (TMUTCD) :
https://mutcd.fhwa.dot.gov/resources/state_info/texas/tx.htm

PAVEMENT SYSTEMS

GENERAL NOTES

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

302,303

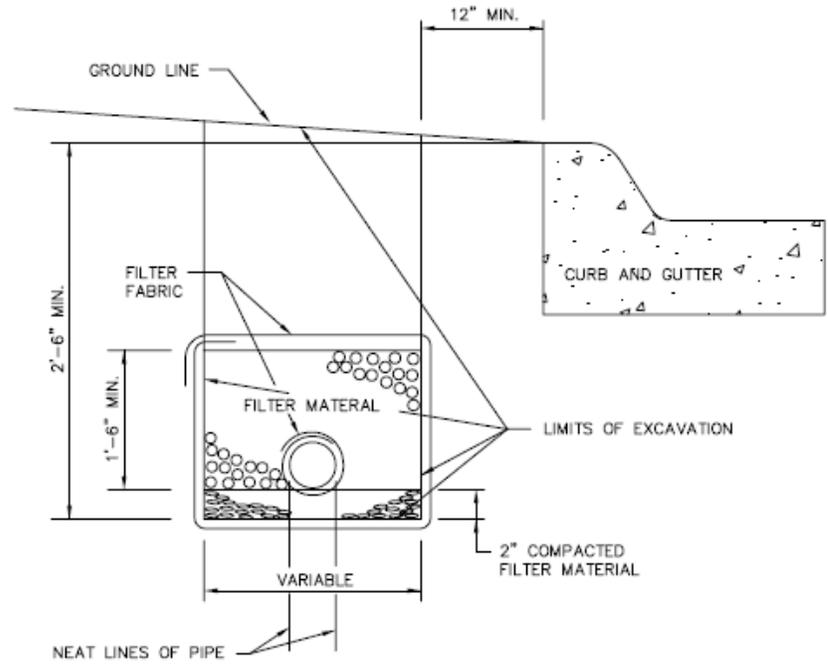
DATE

OCT. '04

STANDARD DRAWING NO.

~~2190~~

2115



SECTION
N.T.S.

LIMITS OF EXCAVATION

DEPTH OF TRENCH (FT.)	DIST. IN FT. OUTSIDE NEAT LINES OF PIPE SUBDRAIN
0 TO 6	1.00
6 TO 10	1.50
10 TO 15	2.00
OVER 15	2.50

FILTER MATERIAL SPECIFICATIONS

SIEVE SIZE	PERCENTAGE RETAINED ON SIEVE	
	TYPE A	TYPE B
1 1/2	---	0 - 10
3/4	0 - 10	20 - 40
3/8	15 - 35	---
NO. 4	35 - 55	40 - 60

MATERIAL FINER THAN NO. 4 SIEVE

4	---
20	35 - 65
50	75 - 100

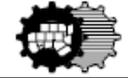
TYPES OF PIPE ACCEPTABLE FOR USE AS SUBDRAIN

1. PERFORATED CORRUGATED METAL PIPE.
2. PERFORATED PVC PIPE.
3. PERFORATED POLYETHYLENE PIPE.

SUBDRAINS

PAVEMENT SUBGRADE

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

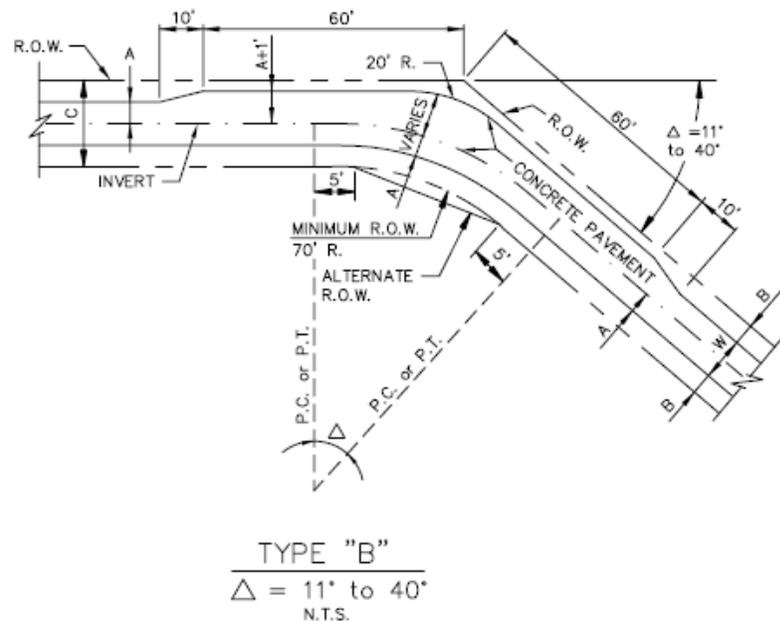
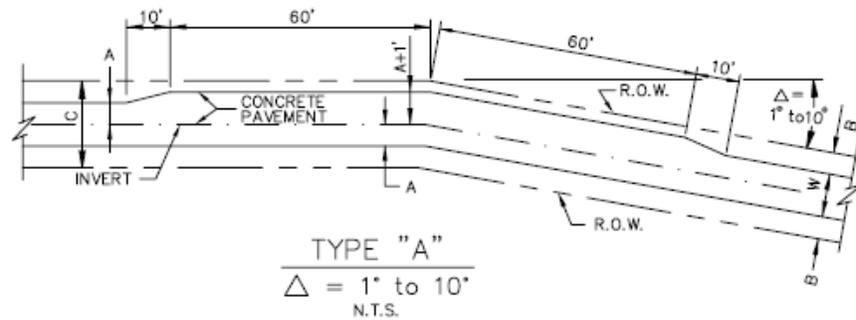
301

DATE

OCT. '04

STANDARD DRAWING NO.

2200



NOTES:

1. DIMENSIONS W, C, A, AND B SHALL BE SPECIFIED ON THE PLANS IN ACCORDANCE WITH STD. DWG. NO. 2040.

ALLEY GEOMETRICS

TYPE "A" & TYPE "B"

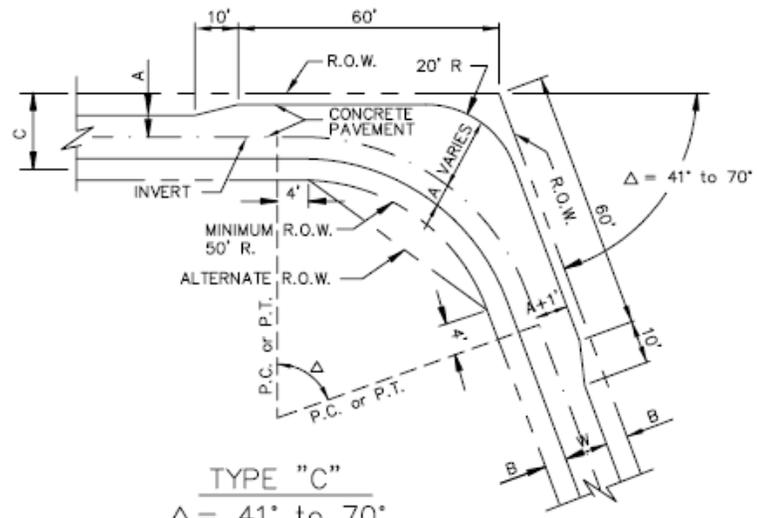
North Central Texas Council of Governments



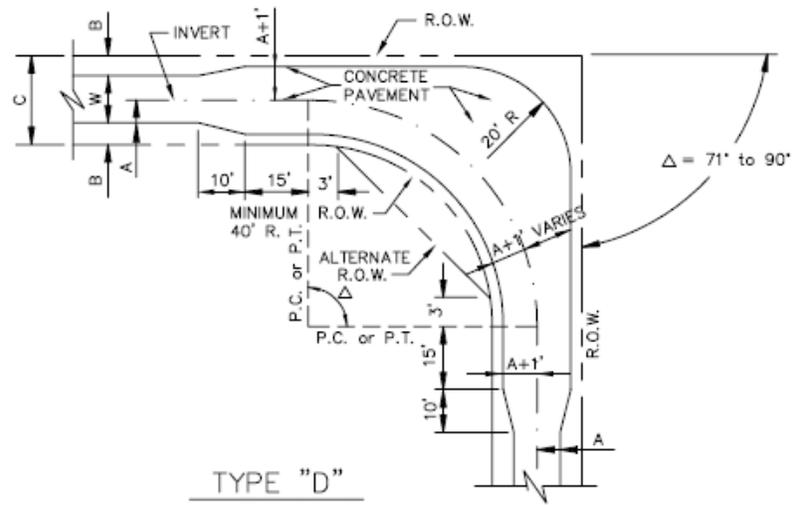
STANDARD SPECIFICATION REFERENCE
 303.5

DATE
 OCT. '04

STANDARD DRAWING NO.
 2210



TYPE "C"
 $\Delta = 41^\circ \text{ to } 70^\circ$
 N.T.S.



TYPE "D"
 $\Delta = 71^\circ \text{ to } 90^\circ$
 N.T.S.

NOTES:

1. DIMENSIONS W, C, A, AND B SHALL BE SPECIFIED ON THE PLANS IN ACCORDANCE WITH STD. DWG. NO. 2040.

ALLEY GEOMETRICS

TYPE "C" & TYPE "D"

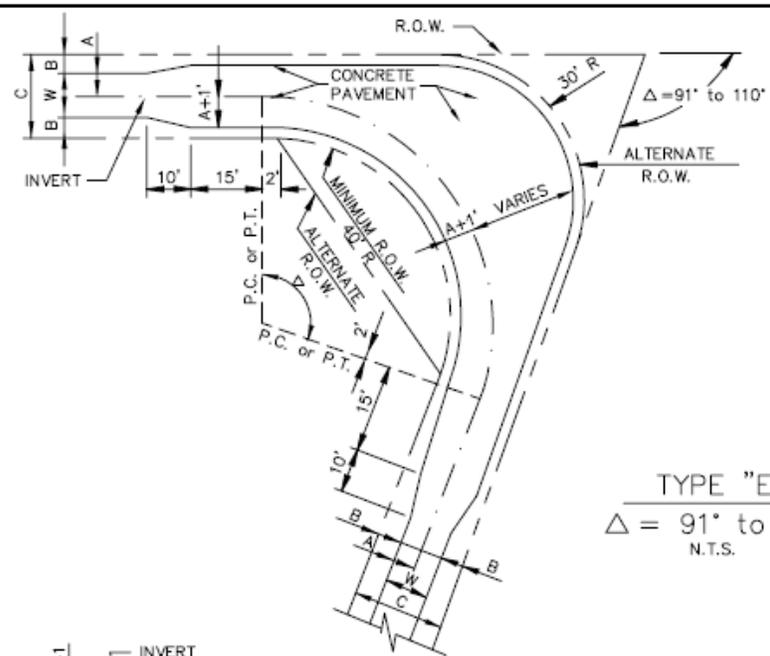
North Central Texas Council of Governments



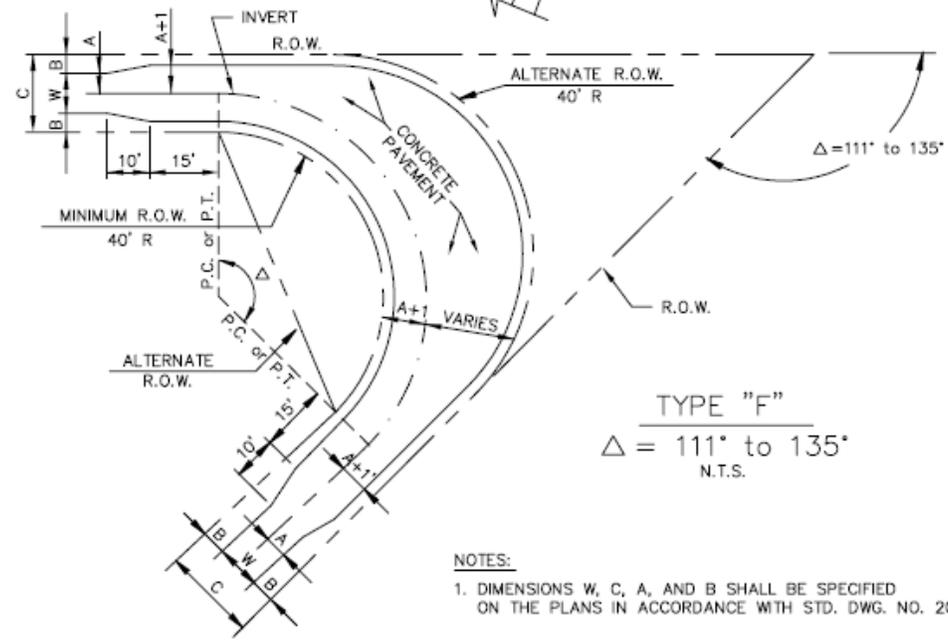
STANDARD SPECIFICATION REFERENCE
 303.5

DATE
 OCT. '04

STANDARD DRAWING NO.
 2220



TYPE "E"
 $\Delta = 91^\circ \text{ to } 110^\circ$
 N.T.S.



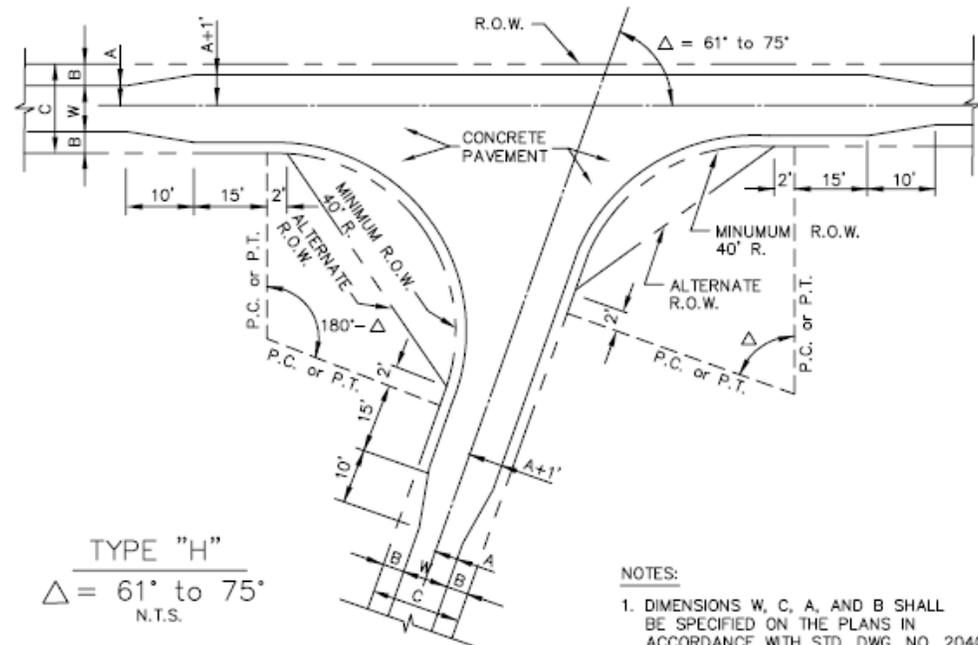
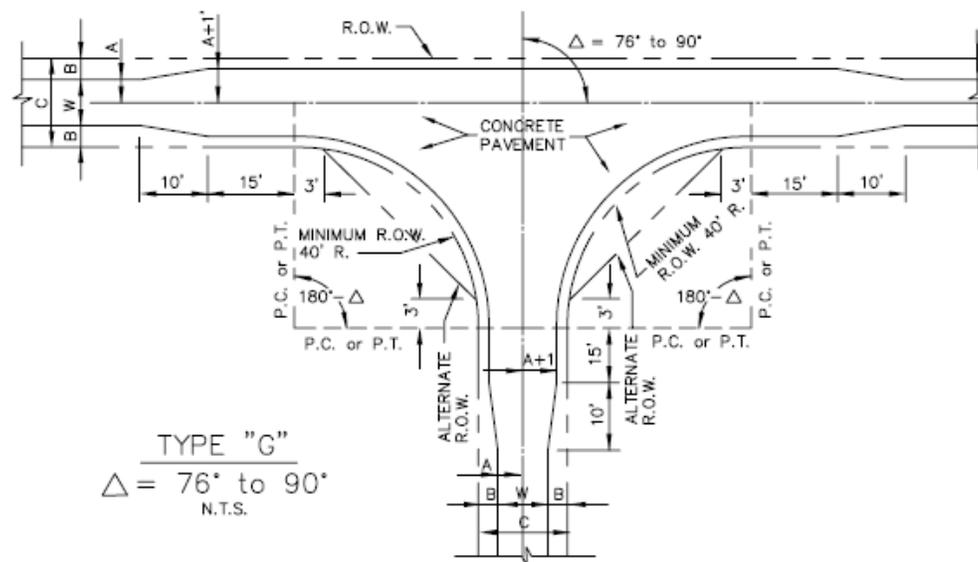
TYPE "F"
 $\Delta = 111^\circ \text{ to } 135^\circ$
 N.T.S.

NOTES:
 1. DIMENSIONS W, C, A, AND B SHALL BE SPECIFIED ON THE PLANS IN ACCORDANCE WITH STD. DWG. NO. 2040.

ALLEY GEOMETRICS
 TYPE "E" & TYPE "F"



STANDARD SPECIFICATION REFERENCE	
303.5	
DATE	STANDARD DRAWING NO.
OCT. '04	2230



NOTES:
 1. DIMENSIONS W, C, A, AND B SHALL BE SPECIFIED ON THE PLANS IN ACCORDANCE WITH STD. DWG. NO. 2040.

ALLEY GEOMETRICS

TYPE "G" & TYPE "H"

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

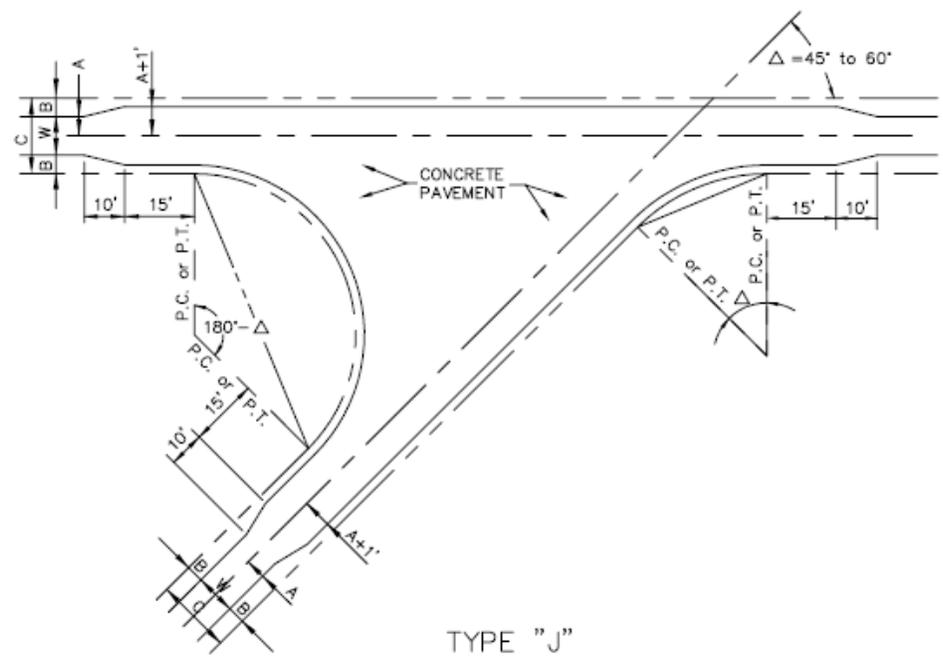
303.5

DATE

STANDARD DRAWING NO.

OCT. '04

2240



TYPE "J"
 $\Delta = 45^\circ \text{ to } 60^\circ$
 N.T.S.

NOTES:

1. DIMENSIONS W, C, A, AND B SHALL BE SPECIFIED ON THE PLANS IN ACCORDANCE WITH STD. DWG. NO. 2040.

ALLEY GEOMETRICS

TYPE "J"

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

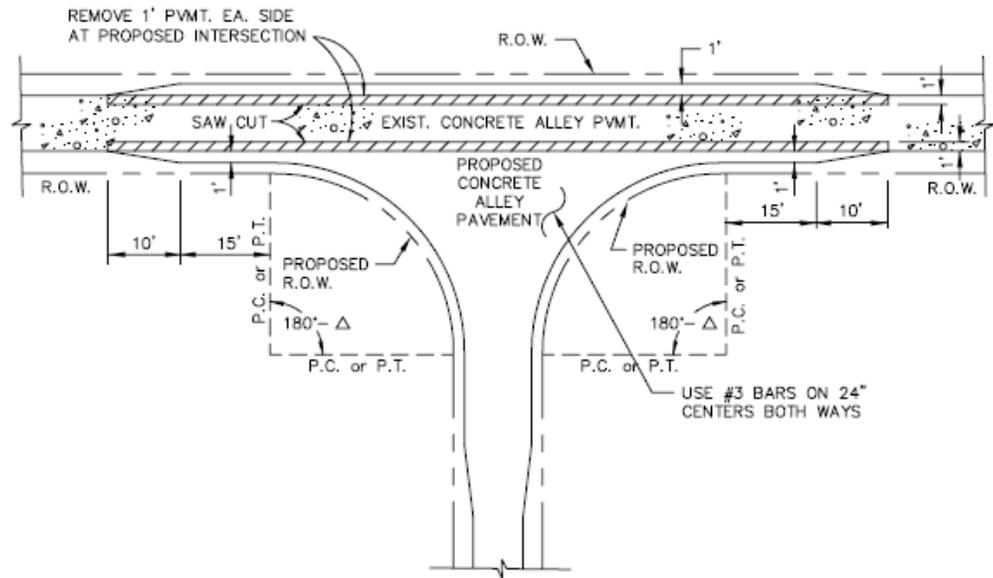
303.5

DATE

OCT. '04

STANDARD DRAWING NO.

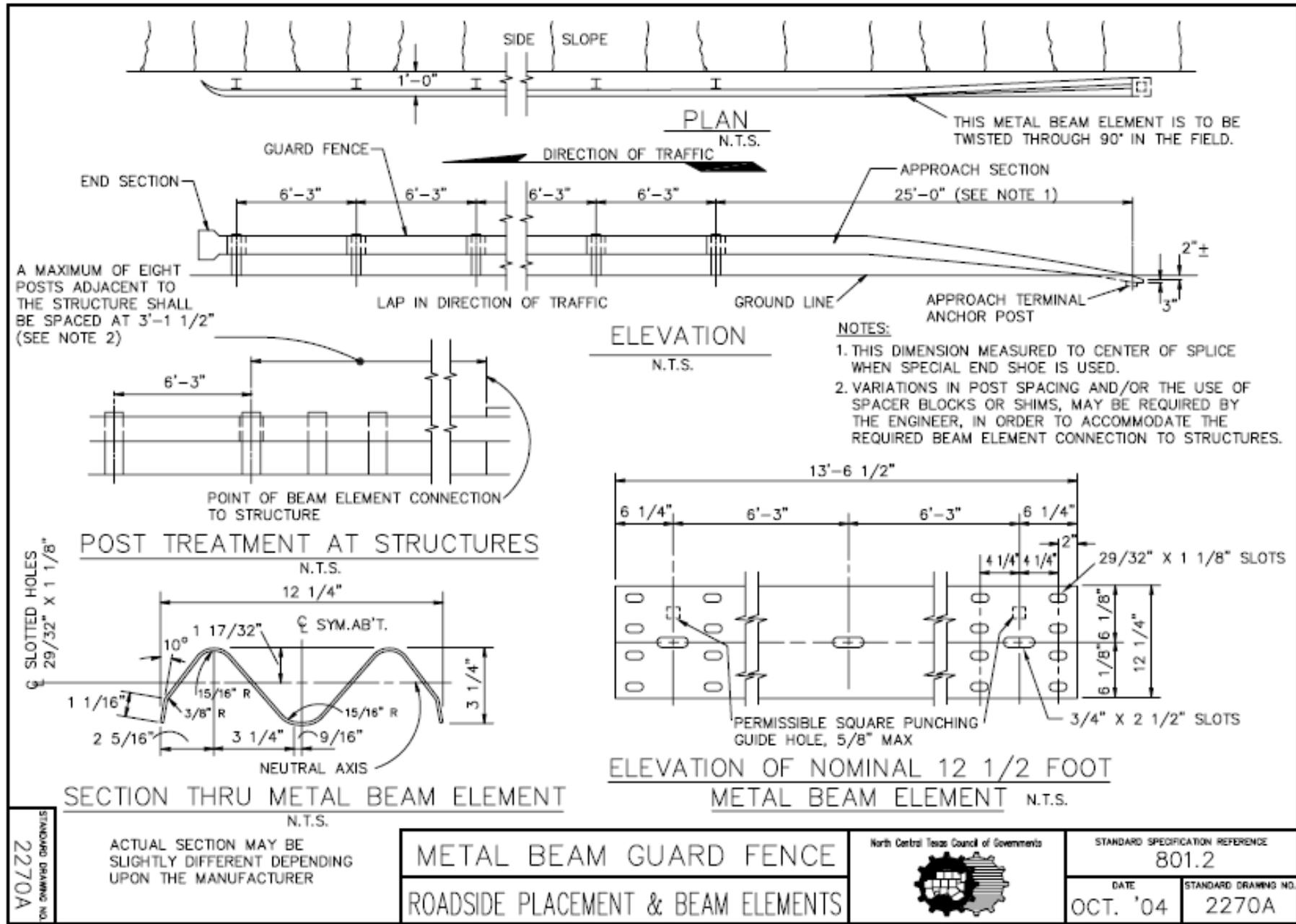
2250



INTERSECTION OF PROPOSED ALLEY
 WITH EXISTING ALLEY PAVEMENT
 N.T.S.

NOTE:
 GEOMETRICS OF PROPOSED ALLEY SHALL
 BE SHOWN ON THE PLANS IN ACCORDANCE
 WITH TYPE "G", "H", OR "J".

ALLEY INTERSECTION PROPOSED TO EXISTING	 North Central Texas Council of Governments	STANDARD SPECIFICATION REFERENCE	
		303.5	
		DATE	STANDARD DRAWING NO.
		OCT. '04	2260



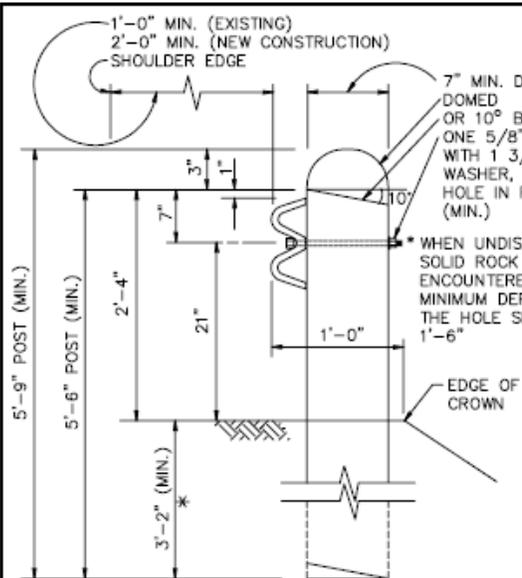
STANDARD DRAWING NO. 2270A

ACTUAL SECTION MAY BE SLIGHTLY DIFFERENT DEPENDING UPON THE MANUFACTURER

METAL BEAM GUARD FENCE
ROADSIDE PLACEMENT & BEAM ELEMENTS

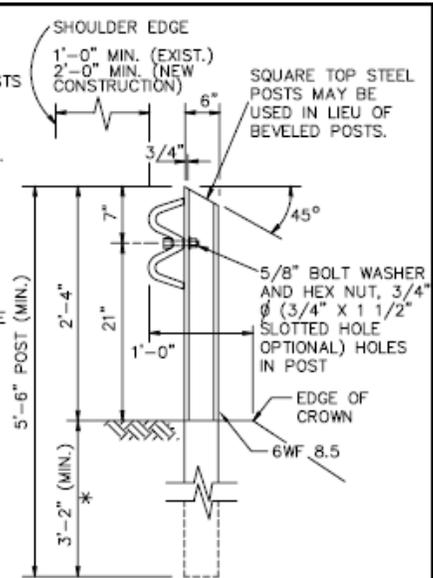


STANDARD SPECIFICATION REFERENCE	
801.2	
DATE	STANDARD DRAWING NO.
OCT. '04	2270A



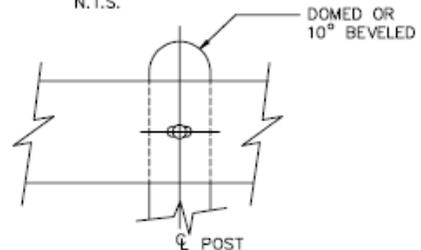
WOOD LINE POST

N.T.S.



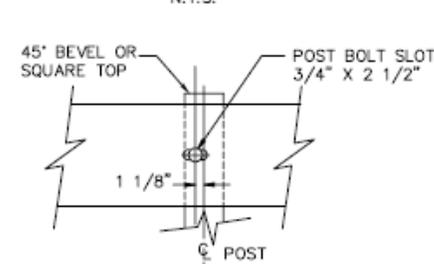
STEEL LINE POST

N.T.S.



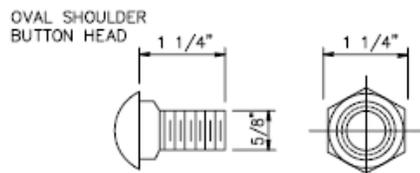
WOOD POST CONNECTION

WOOD POST MAY BE DOMED OR BEVELED.
N.T.S.



STEEL POST CONNECTION

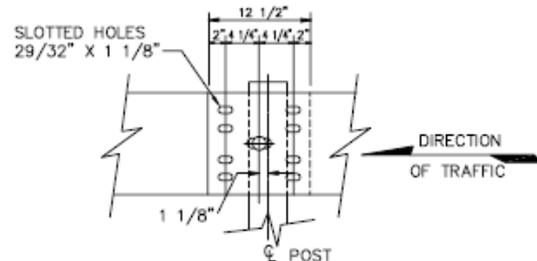
N.T.S.



ANCHOR OR SPLICE BOLT 5/8" NUT
POST BOLT: SIMILAR EXCEPT LENGTH.

(7/8" HEX BOLTS REQUIRED FOR SPECIAL END SHOE)

N.T.S.



BEAM ELEMENT SPLICE

N.T.S.

METAL BEAM GUARD FENCE

LINE POST & CONNECTIONS

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

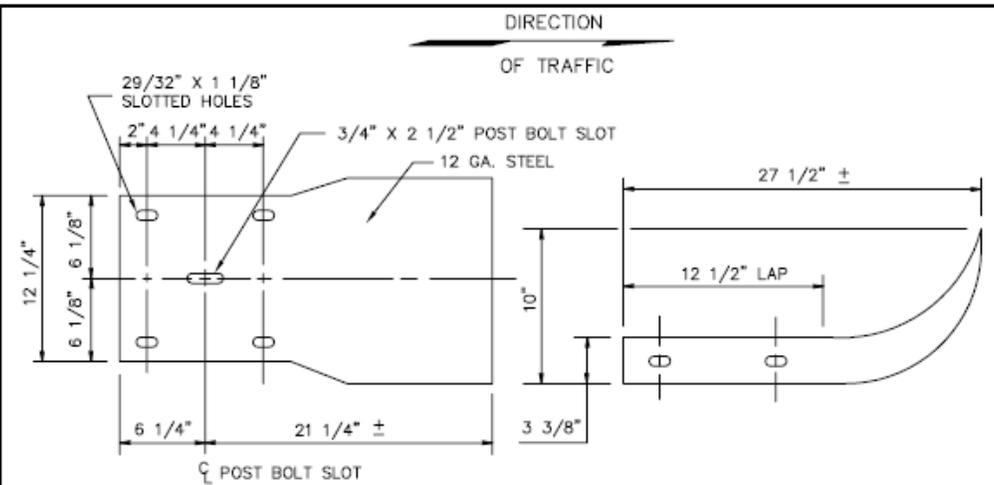
801.2

DATE

OCT. '04

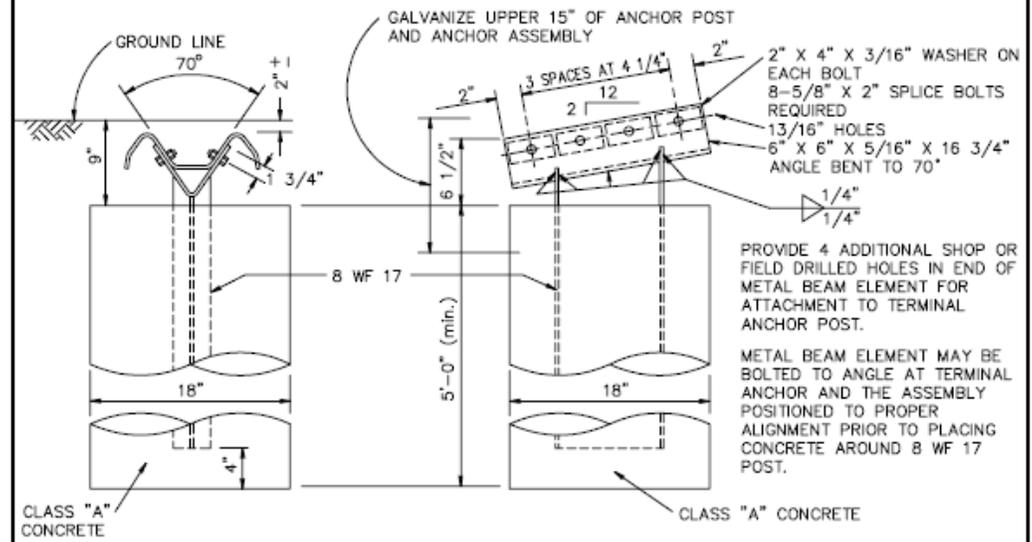
STANDARD DRAWING NO.

2270B



END SECTION – AWAY FROM DIRECTION OF TRAFFIC

N.T.S.



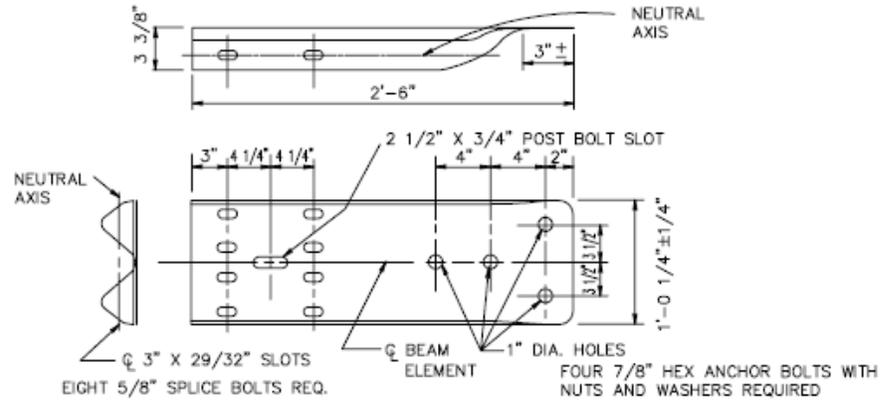
TERMINAL ANGLE ANCHOR POST

N.T.S.

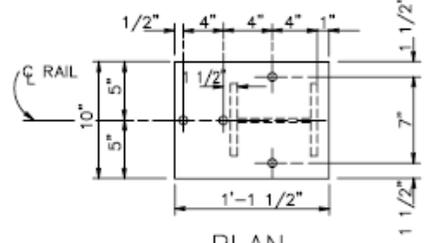
METAL BEAM GUARD FENCE
END SECTION & ANGLE ANCHOR POST



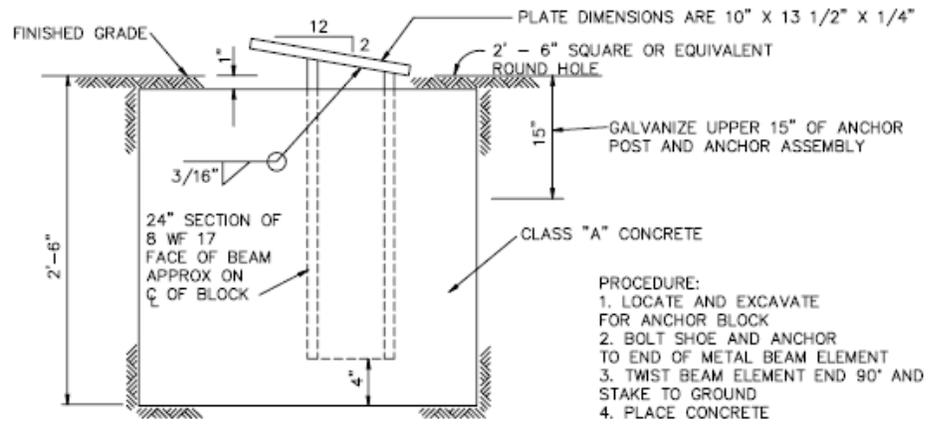
STANDARD SPECIFICATION REFERENCE	
801.2	
DATE	STANDARD DRAWING NO.
OCT. '04	2270C



SPECIAL END SHOE
N.T.S.



PLAN
N.T.S.



- PROCEDURE:
1. LOCATE AND EXCAVATE FOR ANCHOR BLOCK
 2. BOLT SHOE AND ANCHOR TO END OF METAL BEAM ELEMENT
 3. TWIST BEAM ELEMENT END 90° AND STAKE TO GROUND
 4. PLACE CONCRETE

ELEVATION
N.T.S.

SPECIAL END SHOE ANCHOR POST

METAL BEAM GUARD FENCE

SPECIAL END SHOE & ANCHOR POST

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE
801.2

DATE
OCT. '04

STANDARD DRAWING NO.
2270D

METAL BEAM GUARD FENCE
GENERAL NOTES

1. EXCEPT WHERE USED AT STRUCTURES THAT ARE NARROWER THAN CROWN WIDTH OR WHERE OTHERWISE INDICATED ON PLANS, THE FACE OF THE GUARD FENCE SHALL BE LOCATED A MINIMUM OF ONE FOOT FROM THE SHOULDER EDGE ON EXISTING ROADWAYS AND A MINIMUM OF TWO FEET FROM THE SHOULDER EDGE ON NEW CONSTRUCTION. THE EXACT POSITION SHALL BE AS SHOWN ELSEWHERE ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BEAM ELEMENTS SHALL BE TRANSITIONED TO A SMOOTH CONNECTION WITH OTHER STRUCTURES OR BEAM ELEMENTS AS SHOWN ELSEWHERE ON PLANS.
2. AT THE OPTION OF THE CONTRACTOR THE METAL BEAM ELEMENTS FOR THE GUARD FENCE MAY BE FURNISHED IN EITHER 12 1/2 OR 25 FOOT NOMINAL LENGTHS. BEAM ELEMENTS SHALL BE FURNISHED WITH POST BOLT SLOTS FOR 5/8" DIAMETER BOLT CONNECTIONS TO POSTS.
3. BOLTS SHALL BE OF SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT.
4. THE TOP OF THE TERMINAL ANCHOR POST ASSEMBLY AND ALL STEEL FITTINGS THEREON SHALL BE GALVANIZED AS SHOWN.
5. WHERE ROCK IS ENCOUNTERED OR WHERE SHOWN ON THE PLANS, THE DIAMETER OF THE HOLES AND THE MATERIAL FOR BACKFILLING SHALL BE AS DIRECTED BY THE ENGINEER. TIMBER POSTS SHALL NOT BE SET IN CONCRETE.
6. THE TERMINAL ANCHOR POST SHALL BE SET IN CLASS "A" CONCRETE. CONCRETE SHALL BE SUBSIDIARY TO THE BID ITEM "METAL BEAM GUARD FENCE."
7. TIMBER POSTS MAY BE BEVELED AT APPROXIMATELY 10 DEGREES ON THE TOP OR BOTH ENDS WITH HIGH SIDE OF TOP OF POST PLACED TOWARD THE ROADWAY OR THEY MAY BE DOMED.
8. AN ANCHOR OTHER THAN TO A TERMINAL ANCHOR POST SHALL CONSIST OF A CONNECTION SIMILAR TO THE BEAM ELEMENT SPLICE OR SIMILAR TO THE SPECIAL END SHOE.
9. SPECIAL FABRICATION WILL BE REQUIRED IN INSTALLATIONS HAVING A CURVATURE OF LESS THAN 150' RADIUS.
10. WOOD POSTS MUST BE TREATED IN MANNER APPROVED BY THE ENGINEER.
11. THE SPECIAL END SHOE ANCHOR MAY BE USED WITH THE 18" X 5'-0" CONCRETE FOOTING OR THE ANGLE ANCHOR MAY BE USED WITH THE 2'-6" SQUARE OR EQUIVALENT CONCRETE FOOTING.
12. ALL METAL ELEMENTS WILL BE 12 GAUGE STEEL UNLESS STATED OTHERWISE ON PLANS.

METAL BEAM GUARD FENCE
GENERAL NOTES

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

801.2

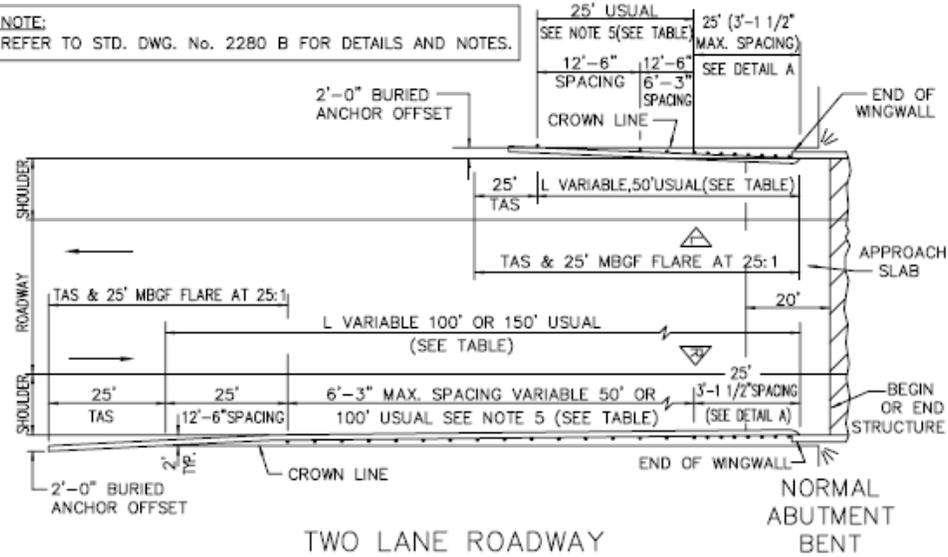
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2270E

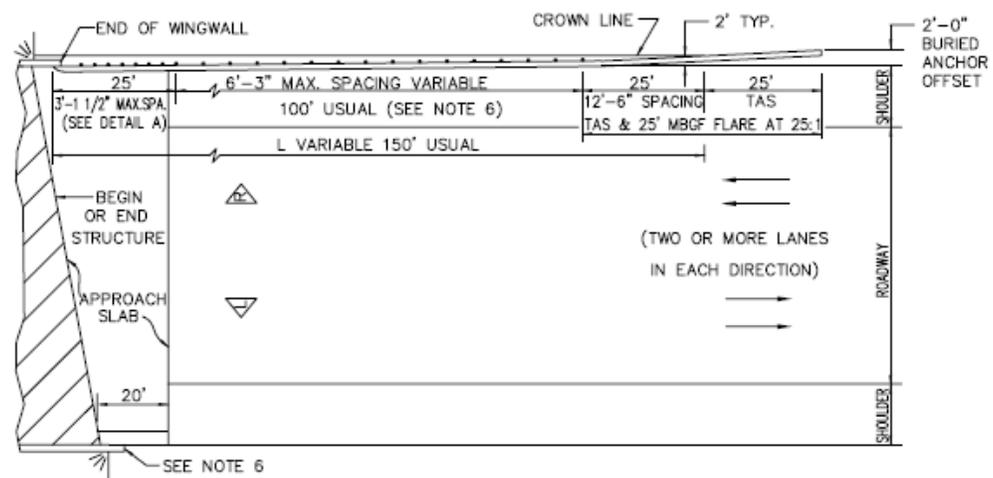
NOTE:
REFER TO STD. DWG. No. 2280 B FOR DETAILS AND NOTES.



TWO LANE ROADWAY

N.T.S.

TAS: TERMINAL ANCHOR SECTION



MULTILANE UNDIVIDED ROADWAY

N.T.S.

CROWN WIDTH BRIDGE

(SEE NOTE 7 FOR RESTRICTIVE WIDTH BRIDGE)

METAL BEAM GUARD FENCE

TWO-WAY TRAFFIC BRIDGE END

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

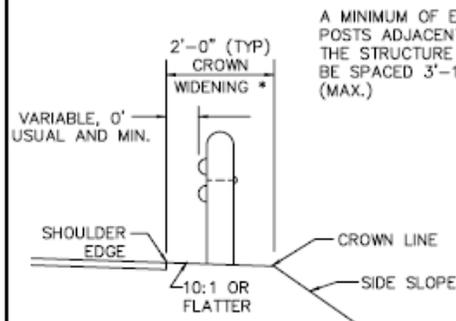
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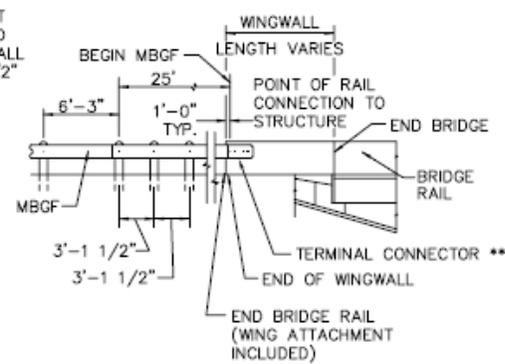
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TYPICAL CROSS SECTION

N.T.S.

* APPLIES TO CONSTRUCTION ON NEW ALIGNMENT OR WHERE EXISTING ROADWAY CROSS SECTION IS TO BE WIDENED TO INCREASE ROADWAY WIDTH. DOES NOT APPLY TO REHABILITATION WORK WHERE EXISTING ROADWAY CROWN WIDTH IS TO BE RETAINED.



POST TREATMENT AT STRUCTURES
DETAIL A

N.T.S.

** TYPICAL CONNECTION—SEE BRIDGE RAIL OR OTHER PLAN SHEETS FOR DETAILS OF MBGF TO BRIDGE RAIL CONNECTION.

LENGTH Ⓞ OF NEED, L, FT.

TWO LANE HIGHWAYS				MULTILANE UNDIVIDED HWYS.	
750 or less ADT	more than 750 ADT	all ADT's		all ADT's	
◀ side	▶ side	◀ side	▶ side	◀ side	▶ side
50 Ⓞ	100	50 Ⓞ	150	0	150

Ⓞ LENGTHS ARE FOR TYPICAL CROSS SECTIONAL & PLACEMENT CONDITIONS. FOR UNUSUAL CONDITIONS, A CUSTOM DESIGN SHOULD BE DEVELOPED.

◀ INDICATES LEFT SIDE OF TRAFFIC APPROACHING BRIDGE.

▶ INDICATES RIGHT SIDE OF TRAFFIC APPROACHING BRIDGE.

DESIGN NOTES:

- THE T.A.S. AND TYPICALLY ADJACENT 25' MBGF SHOULD BE FLARED FROM THE SHOULDER EDGE AT 25:1 TO PROVIDE A 2' USUAL OFFSET TO BURIED ANCHOR.
- WHERE LENGTH (L) OF MBGF IS 50 FEET, POST SPACING SHALL BE AS DETAILED HEREON (SEE PLAN LAYOUT FOR TWO LANE (RURAL) HIGHWAY), LEFT SIDE OF TRAFFIC APPROACHING BRIDGE. WHERE LENGTH (L) OF MBGF IS 75 FEET OR MORE, POST SPACING SHALL BE 3'-1 1/2" FOR THE 25' SECTION ADJACENT TO THE BRIDGE, 12'-6" FOR THE 25' SECTION ADJACENT TO THE T.A.S. AND 6'-3" FOR THE REMAINING INTERVENING LENGTH.
- THE SLOPE BETWEEN THE CROWN LINE AND OUTSIDE EDGE OF SHOULDER SHOULD BE 10:1 OR FLATTER. THE CROWN SHOULD BE WIDENED TO ACCOMMODATE MBGF. TYPICALLY THE CROWN LINE SHOULD BE 2 FEET FROM THE OUTSIDE SHOULDER EDGE (SEE TYPICAL CROSS SECTION).
- FOR RESTRICTIVE WIDTH BRIDGES, A 25 FOOT TANGENT SECTION OF MBGF SHOULD CONNECT TO THE WINGWALL. THE ADJOINING MBGF THAT LIES WITHIN THE ROADWAY (LANE & SHOULDER AREAS) CROWN SHOULD BE FLARED AT THE RATE OF 25:1 (LONGITUDINAL:LATERAL). LENGTH SHOULD BE GOVERNED BY TABULATED VALUES OR THE LENGTH NECESSARY TO LOCATE THE BURIED ANCHOR AT A 2-FOOT OFFSET FROM SHOULDER EDGE, WHICHEVER IS GREATER.
- AVERAGE DAILY TRAFFIC (ADT) IS FOR THE CURRENT YEAR. WHERE SIGNIFICANT TRAFFIC VOLUME GROWTH IS ANTICIPATED ON LOW VOLUME (0-750 ADT) HIGHWAYS, USE LENGTHS SHOWN FOR HIGHER VOLUME CATEGORY.
- PROVIDE MINIMUM 50 FT. MBGF PLUS T.A.S. FOR FOUR LANE UNDIVIDED HIGHWAYS. FOR FOUR LANE HIGHWAYS WITH A FLUSH MEDIAN OR FOR HIGHWAYS WITH SIX OR MORE LANES, MBGF IS NOT A REQUIRED BRIDGE END TREATMENT. HOWEVER, OTHER NEARBY HAZARDS MAY WARRANT SHIELDING WITH MBGF.

GENERAL NOTES:

- FOR METAL BEAM GUARD FENCE DETAILS AND METHOD OF TERMINATION, SEE STD. DWGS. No. 2270A - 2270E.
- VARIATIONS IN POST SPACINGS AND/OR THE USE OF SPACER BLOCKS OR SHIMS MAY BE REQUIRED BY THE ENGINEER IN ORDER TO ACCOMMODATE THE REQUIRED BEAM ELEMENT CONNECTION TO STRUCTURES.
- QUANTITIES OF METAL BEAM GUARD FENCE (MBGF) AT INDIVIDUAL BRIDGE ENDS ARE SHOWN ELSEWHERE IN THE PLANS.

METAL BEAM GUARD FENCE

TWO-WAY TRAFFIC BRIDGE END

North Central Texas Council of Governments



STANDARD SPECIFICATION REFERENCE

801.2

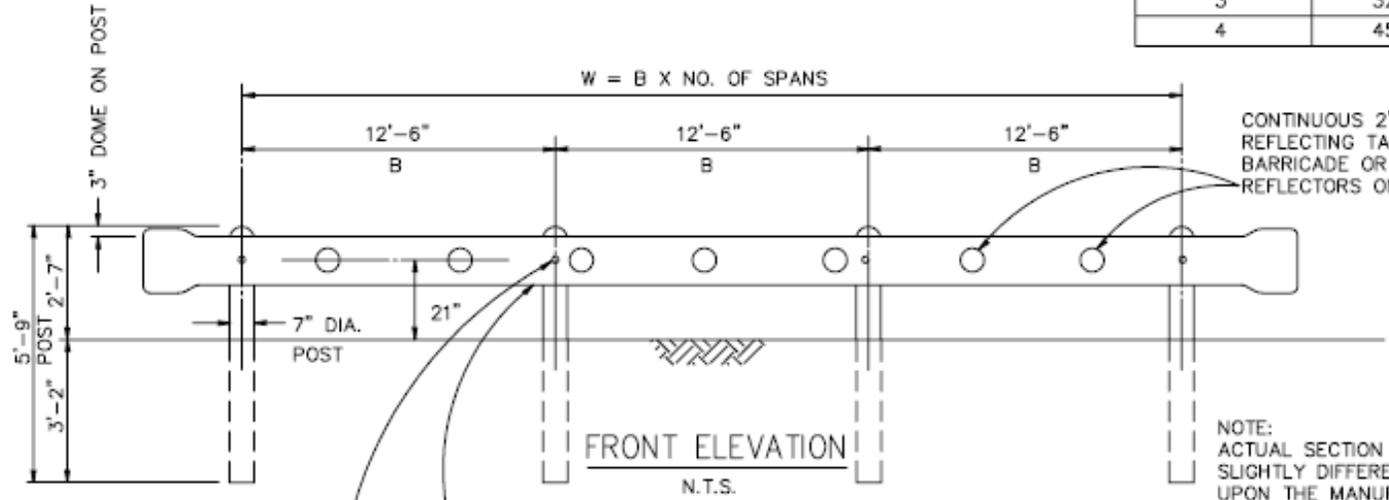
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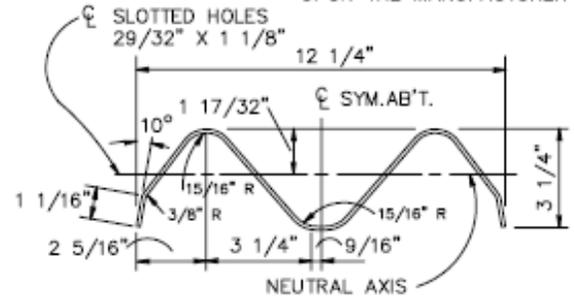
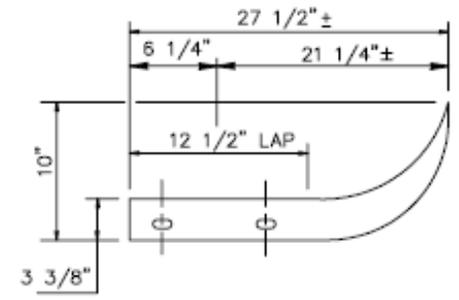
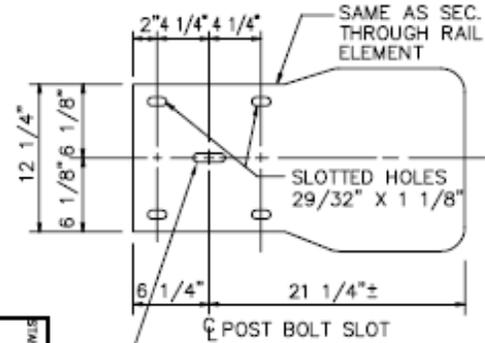
NO. OF SPANS	ROADWAY SECTION WIDTH
2	18' to 31'
3	32' to 44'
4	45' to 56'



FASTEN TO POST WITH ONE 5/8" BOLT WITH 1 3/4" O.D. WASHER BEHIND POST, 7/8" Ø HOLE IN POST.

METAL BEAM GUARD FENCE SHALL BE GALVANIZED STEEL (12 GA.)

NOTE: ACTUAL SECTION MAY BE SLIGHTLY DIFFERENT DEPENDING UPON THE MANUFACTURER



STANDARD DRAWING NO. 2290

METAL BEAM BARRICADE
END OF ROAD



STANDARD SPECIFICATION REFERENCE	
801.2	
DATE	STANDARD DRAWING NO.
OCT. '04	2290

Next Steps

- Determine action items for Subcommittee Members and NCTCOG staff

Next Standard Drawings Meetings

June 15, 2020
June 8th?
10am-11:30am
UberConference