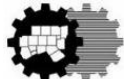


PROJECT STATUS REPORT

Surface Transportation Technical Committee

April 28, 2023



North Central Texas
Council of Governments
Transportation Department

BACKGROUND

- MPO Milestone Policy
 - Focuses on projects funded 10+ years that have not gone to construction
 - Creates agency accountability to implement projects
- FY2023 Project Tracking
 - Focuses on projects slated for implementation in FY2023
 - Highlights potential problems in order to prevent delays
 - Provides earlier monitoring
 - Enables the Regional Transportation Council (RTC) to take corrective actions to avoid accumulation of carryover balances

MPO MILESTONE POLICY ROUND 2 STATUS UPDATE

SUMMARY OF PROJECTS THAT HAVE NOT GONE TO CONSTRUCTION (APRIL 2023)¹

PROJECT CATEGORIES	NUMBER OF PROJECTS	TOTAL FUNDING OF PROJECTS
Scheduled Letting FY2023	10	\$95,451,136
Scheduled Letting FY2024 or Beyond	3	\$201,816,145
Total	13	\$297,267,281

1: To date, seven projects have let by their established deadlines and three have been removed from the TIP at the request of the implementing agency.

PROJECT RISK BY FISCAL YEAR

PROJECT CATEGORY	PROJECT RATING		
	Green (Low Risk of Delay)	Yellow (Medium Risk of Delay)	Red (High Risk of Delay)
Scheduled Letting FY2023	0	0	10
Scheduled Letting FY2024 or Beyond	1	2	0
TOTAL	1	2	10

PROPOSED CHANGES TO THE MILESTONE POLICY AND PROJECT TRACKING PROCESS

- Quarterly updates have been provided to the Committee and RTC regarding the status of MPO Milestone Policy projects, along with separate updates on the progress of project phases in the current fiscal year.
- The process has led to an increase in communication between project sponsors, NCTCOG, and TxDOT, but there are few substantive updates to report due to the reporting frequency and the time a project takes to be implemented.
- Therefore, staff is proposing to adjust the reporting frequency for the Milestone Policy and Project Tracking initiatives from four times a year to at least two times a year. The items will also continue to be combined into one.

FISCAL YEAR 2023 PROJECT TRACKING

SUMMARY OF TIP FY2023 PROJECT FUNDING - CMAQ

	NOVEMBER 2022	APRIL 2023
Total Federal Funding Programmed ¹	\$85,866,811	\$103,095,327
Federal Funding Obligated (2023) ^{2, 3}	(\$515,153)	\$578,507
FY2023 Project Phases	36	39
Project Phases Obligated to Date	0	3

1: Programmed funding is comprised of what is included in the TIP as well as transactions that have not formally been made in the TIP (e.g., early obligations or delaying projects to future years)

2: Obligations based on the federal fiscal year, which runs from October to September

3: Obligation amounts as of 4/14/2023

SUMMARY OF TIP FY2023 PROJECT FUNDING - STBG

	NOVEMBER 2022	APRIL 2023
Total Federal Funding Programmed ¹	\$230,670,388	\$233,671,714
Federal Funding Obligated (2023) ^{2, 3}	(\$1,405,523)	\$23,661,006
FY2023 Project Phases	77	75
Project Phases Obligated to Date	0	12

1: Programmed funding is comprised of what is included in the TIP as well as transactions that have not formally been made in the TIP (e.g., early obligations or delaying projects to future years)

2: Obligations based on the federal fiscal year, which runs from October to September

3: Obligation amounts as of 4/14/2023

SUMMARY OF TIP FY2023 PROJECT FUNDING – TRANSPORTATION ALTERNATIVES SET ASIDE

	NOVEMBER 2022	APRIL 2023
Total Federal Funding Programmed ¹	\$19,234,907	\$20,863,858
Federal Funding Obligated (2023) ^{2, 3}	(\$1,674,876)	(\$1,994,510)
FY2023 Project Phases	18	19
Project Phases Obligated to Date	0	2

1: Programmed funding is comprised of what is included in the TIP as well as transactions that have not formally been made in the TIP (e.g., early obligations or delaying projects to future years)

2: Obligations based on the federal fiscal year, which runs from October to September

3: Obligation amounts as of 4/14/2023

REQUESTED ACTION

- US 81/287 Corridor
 - Projects will most likely miss the original letting deadline at the end of FY2023
 - Staff recommends that the RTC extend the project's deadline to the end of FY2025 and leave the funding on the project. The projects will be monitored to ensure that they proceed to construction.
- Corporate Drive
 - Four segments of the project will most likely miss their letting deadline at the end of FY2023. One is already under construction.
 - Two (TIP 20131 and 20152) are anticipated to let in November 2023 and two (TIP 20108 and 20111) are anticipated to let in April 2024.
 - Staff recommends that the RTC extend the projects' deadlines to match the new let dates and leave the funding on the projects. They will be monitored to ensure that they proceed to construction.
- Adjust the reporting frequency for Milestone Policy and Project Tracking Updates from four times a year to at least two times a year
- Administratively amending the Transportation Improvement Program (TIP)/Statewide Transportation Improvement Program (STIP) and other administrative/planning documents as needed

QUESTIONS/COMMENTS?

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Application to the Federal Highway Administration Charging and Fueling Infrastructure Program

Jared Wright
Air Quality Planner

**Surface Transportation Technical
Committee**

April 28, 2023

Charging and Fueling Infrastructure Program

Established by the Bipartisan Infrastructure Law to deploy publicly accessible electric vehicle (EV) charging and other alternative fueling infrastructure in communities and along designated Alternative Fuel Corridors (AFC)

For FY22 and FY23, \$700M split evenly in two funding categories:

Corridor Program (\$350M Available)

- Minimum: \$1,000,000
- No maximum

Community Program (\$350M Available)

- Minimum: \$500,000
- Maximum: \$15,000,000

Federal share: up to 80 percent of the total project cost

Eligible entities that contract with a private entity must include that the private entity will be responsible for the non-Federal share of the project cost

Applications Due May 30, 2023 - <https://www.fhwa.dot.gov/environment/cfi/>

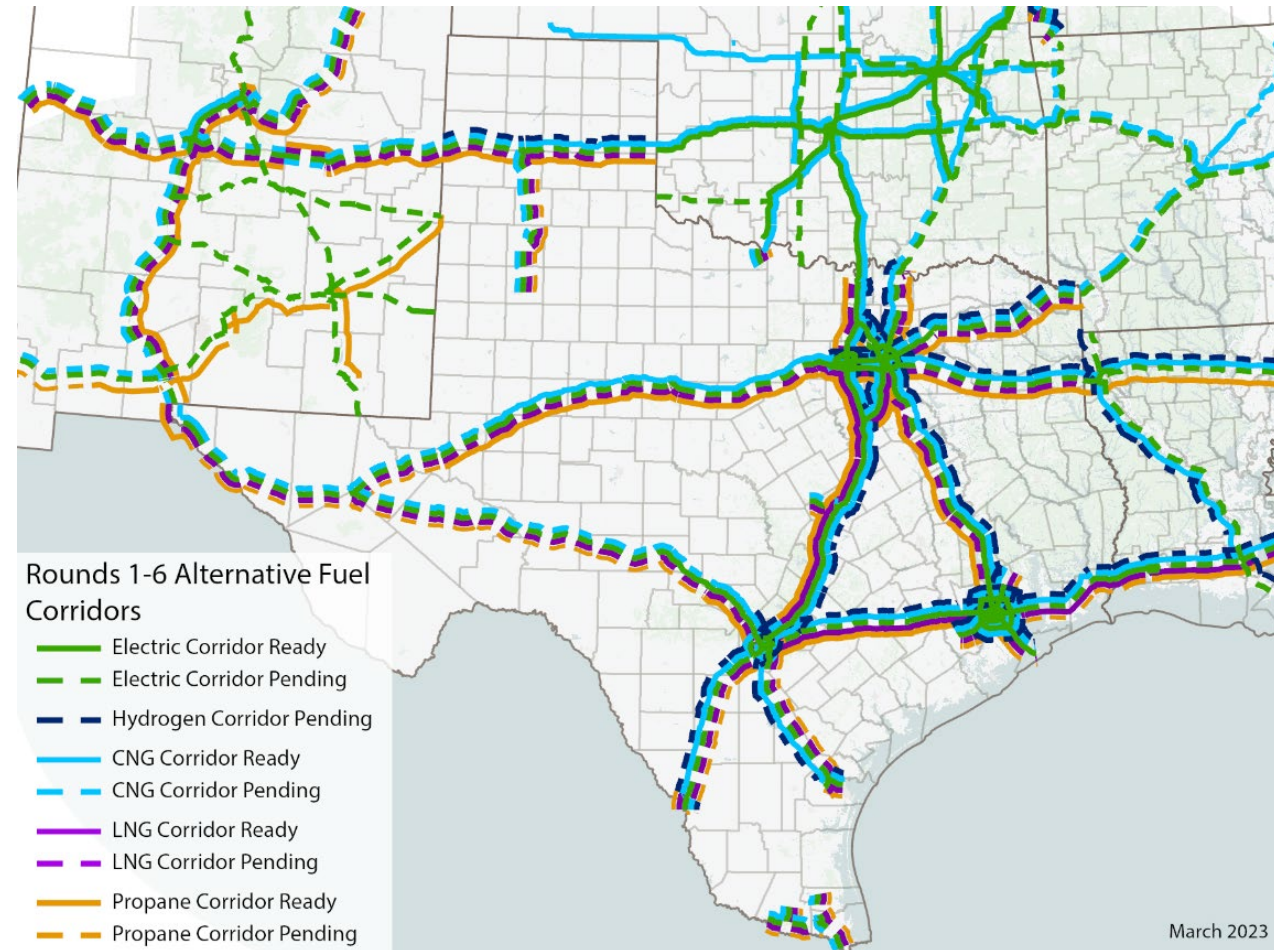


Corridor Program: Eligibility and Need

Designed to build EV charging or alternative fueling infrastructure along designated Alternative Fuel Corridors (AFC)

Opportunity to Start Deploying Recommendations from IH-45 Zero Emission Vehicle Infrastructure Plan

- Battery Electric for Local Freight
- Hydrogen Fuel Cell Electric for Long Haul Freight



Medium/Heavy-Duty Vehicle Impacts:
Only ~5 % of Miles Traveled but Nearly
~40% of Nitrogen Oxides (NO_x)



Corridor Program: Approach

Call for Partners opened on March 6, 2023, to establish public-private partnerships between NCTCOG and project teams to scope zero-emission vehicle (ZEV) infrastructure projects and submit applications to the Charging and Fueling Infrastructure (CFI) Corridor Program and other federal and state funding opportunities

Eligibility Criteria for CFI Proposals:

- Designed to support medium- and heavy-duty ZEVs
- Publicly accessible electric charging and/or hydrogen refueling equipment
- Located along a designated AFC for the proposed fuel
- Impacts the NCTCOG region
- Complies with CFI program eligibility

Selection Criteria:

- Completeness of the project team
- Likelihood of project success
- Availability of matching funds
- Extent of grant-writing resources
- Consistency with IH-45 ZEV Plan

**Received 12 proposals by
deadline of April 7, 2023**



Corridor Program: Recommendations

Coordinate with Proposal Teams to Refine Project Scopes for Federal Highway Administration (FHWA) Proposal:

Hydrogen refueling near AllianceTexas Mobility Innovation Zone, Southern Dallas County Inland Port, and around Texas Triangle

- Up to 7 hydrogen refueling stations
- ~\$11 Million in federal funding estimated per station

Local freight electric truck depot in Denton

- ~\$5 Million in federal funding estimated

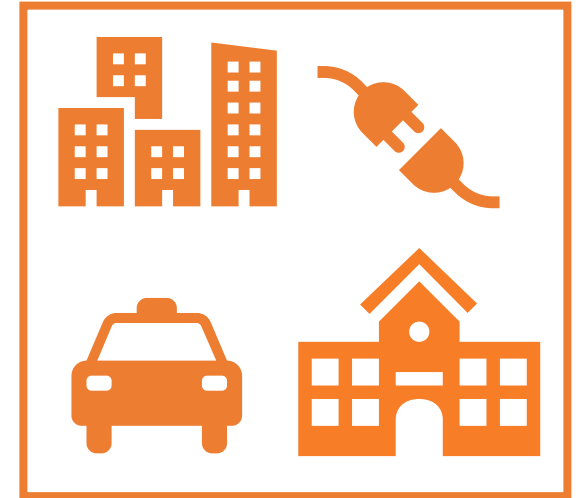


Community Program: Eligibility

Designed to reduce greenhouse gas emissions and to expand or fill gaps in access to infrastructure.

FHWA focus areas include:

- Justice40 environmental justice areas
- Low ratio of private parking to households
- High ratio of multiunit dwellings to single family homes
- Low- and- moderate income neighborhoods
- Multi-modal hubs and shared-use fleets and services
- Fleet vehicles that operate in communities
- Rural areas



Extensive local government interest in building additional infrastructure



Community Program: Approach and Recommendation

Apply on behalf of the region for funding to build up to 100 EV charging stations regionwide

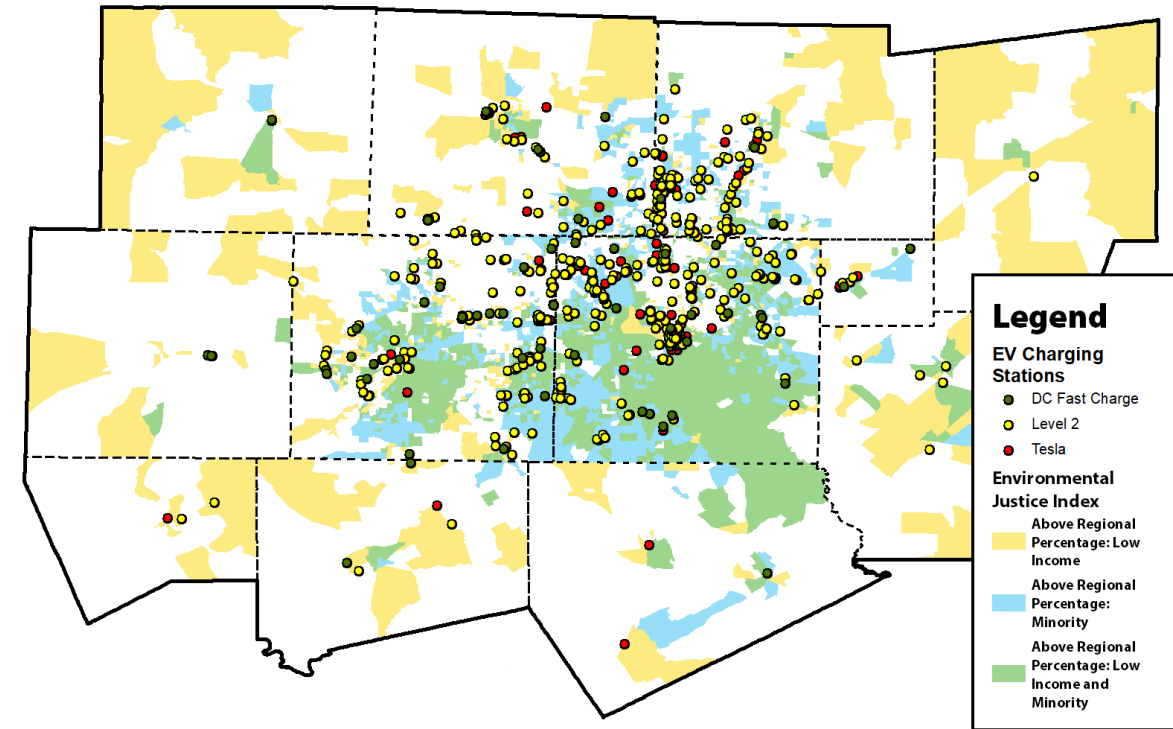
At least 50% in Justice40 Areas
Emphasizing Focus Areas

Create specialized technical teams to streamline project implementation

Pursue maximum award of \$15 Million

Requesting public entities submit letters of support by May 19

NCTCOG Environmental Justice Index and Public EV Charging Stations



Action Requested

Recommend Regional Transportation Council approval of NCTCOG to pursue funding from the Federal Highway Administration Charging and Fueling Infrastructure Discretionary Grant Program for two grant categories:

Corridor Program – Develop an application to build:

Up to 7 Hydrogen Refueling Sites Near Alliance Texas Mobility Innovation Zone, Southern Dallas County Inland Port, and around Texas Triangle

Local Freight Electric Truck Depot in Denton

Community Program – Develop an application to build:

Up to 100 EV Charging Stations Throughout the Region

Demonstrate Effectiveness of Specialized Technical Teams for Implementation



Schedule

Milestone	Date
Corridor Program: Call for Partners Proposal Deadline	April 7, 2023
STTC Recommendation of RTC Approval	April 28, 2023
RTC Approval	May 11, 2023
Deadline for Local Government Letters of Support for Community EV Charging Proposal	May 19, 2023
Executive Board Authorization to Submit Proposals	May 25, 2023
STTC Information on Submitted Application	May 26, 2023
FHWA Application Deadline	May 30, 2023



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FEDERAL AND STATE LEGISLATIVE UPDATE

REBEKAH GONGORA

NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS

Surface Transportation Technical Committee

April 28, 2023

FEDERAL UPDATE

COMMITTEE HEARINGS

House Subcommittee on Highways and Transit

- Met on March 28 to discuss the implementation of the Infrastructure Investment and Jobs Act
- Focused on Buy America challenges, permitting delays, supply chain disruptions, inflation, discretionary grant process, and workforce development

House Transportation and Infrastructure Schedule

- Upcoming hearings on Federal Aviation Administration Reauthorization, Pipeline Safety Reauthorization, Water Resources Development Act for 2024



STATE BUDGET

HB 1 HOUSE

- House Approved \$302.6B Budget on April 6
 - \$136.9B in General Revenue spending
 - \$37.1B for TxDOT
- Article VII – Transportation Amendments included:
 - I-20 Passenger Rail Project \$10M matching funds, if Federal grant awarded
 - UTP 10% Discretionary Cap
 - Federal Funding Report
 - Outreach and Education Programs on Transportation Safety

HB 1 SENATE

- Senate Approved \$308B Budget on April 17
 - \$141.3B in General Revenue spending
 - \$37.4B for TxDOT
- Article VII – Transportation Amendments included:
 - \$10M in Federal funds for Southern Gateway Deck Park Phase 2 Project, if it meets Federal funding requirements/if grant awarded



BILLS OF INTEREST

Transportation Funding

- **SB 505** Would create a \$200/\$400 Electric vehicle registration fee
Approved in Senate, Approved by Full House 4/27
- **SB 1232** Would extend Prop 1 by 8 years **Approved in Senate, Referred to House Appropriations 4/24**
- **SCR 2** Would extend Proposition 7 by 10 years **Approved in Senate, Approved in House Appropriations 4/20**
- **HJR 144** Would authorize the use of money in the State Highway Fund for roadways for seaports, airports, spaceports, land ports of entry, and international bridges. **Approved in House, Referred to Senate Transportation 4/24**



BILLS OF INTEREST

Safety

- **SB 1663** Authority of a city to alter speed limits **Approved in Senate, Approved by House Committee 4/26, HB 2224 Approved in House 4/20**
- **HB 898** Increased penalties for Move Over Law or Slow Down **Approved in House 4/25, Referred to Senate Transportation 4/26**

Temporary Tags

- **HB 718** Issuance of Metal Plates **Approved by House Committee, Placed on House Calendar for 5/1**
- **HB 914** Temp tag tampering penalties **Approved in House, Referred to Senate Criminal Justice 4/13**



BILLS OF INTEREST

Air Quality

- **SB 784** Regulation of emissions by local governments **Approved in Senate, Approved in House Committee 4/18**
- **SB 1017** Authority to regulate an energy source or engine **Approved by Senate and House 4/26**

Electric Vehicles

- **SB 1001** Regulating vehicle charging equipment **Approved in Senate, Approved in House Committee 4/24**
- **SB 1732** Standards for charging stations **Approved in Senate, Approved by House Committee 4/26**



BILLS OF INTEREST

Technology

- **SB 2144** Advanced air mobility technology advisory committee
Approved in Senate, Approved in House 4/27

Broadband

- **SB 1238** Amends and updates current law relating to broadband development
Approved in Senate, Approved in House State Affairs 4/20
- **HB 9/HJR 125** Development and funding of broadband **Approved in House 4/27**



CONTACT US



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NCTCOG Legislative Updates: www.nctcog.org/legislative

Texas Legislature Online: <https://capitol.texas.gov/>



A large pile of scrap tires in a junkyard. In the background, there is a large, rusty metal tank and some green trees under a clear blue sky. The ground is dirt.

NCTCOG PRESENTATION

REGIONAL SCRAP TIRE PROGRAM

SURFACE TRANSPORTATION TECHNICAL COMMITTEE

APRIL 28, 2023

NICK VAN HAASEN

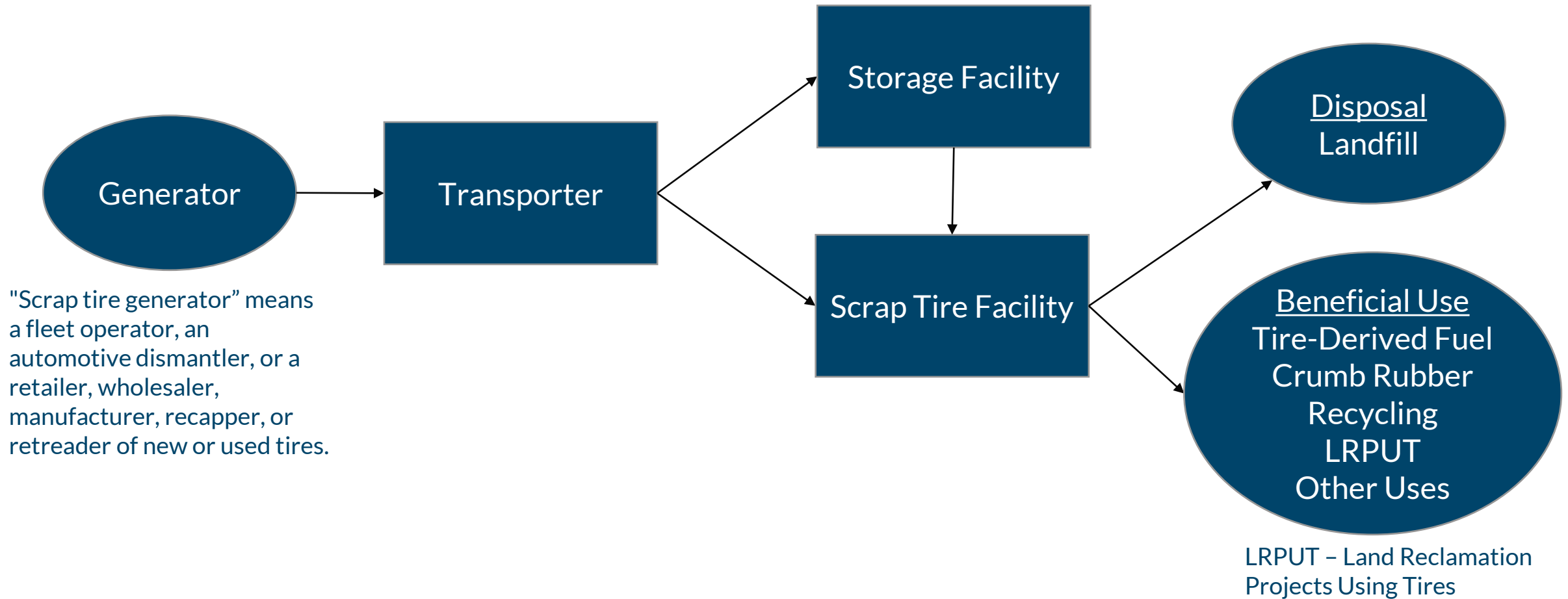


Scrap Tire Challenges

- Expensive and inconvenient for citizens to properly dispose
- “Mom & Pop” shops typically lack the resources for proper disposal of large quantities of tires
- Difficulty with enforcement and prosecution
 - Lack of ordinances
 - Lack of education
- Illegal dumping on roadways, creeks, private property
- Rural areas typically experience more illegal dumping than urban areas
- Lack of adequate markets

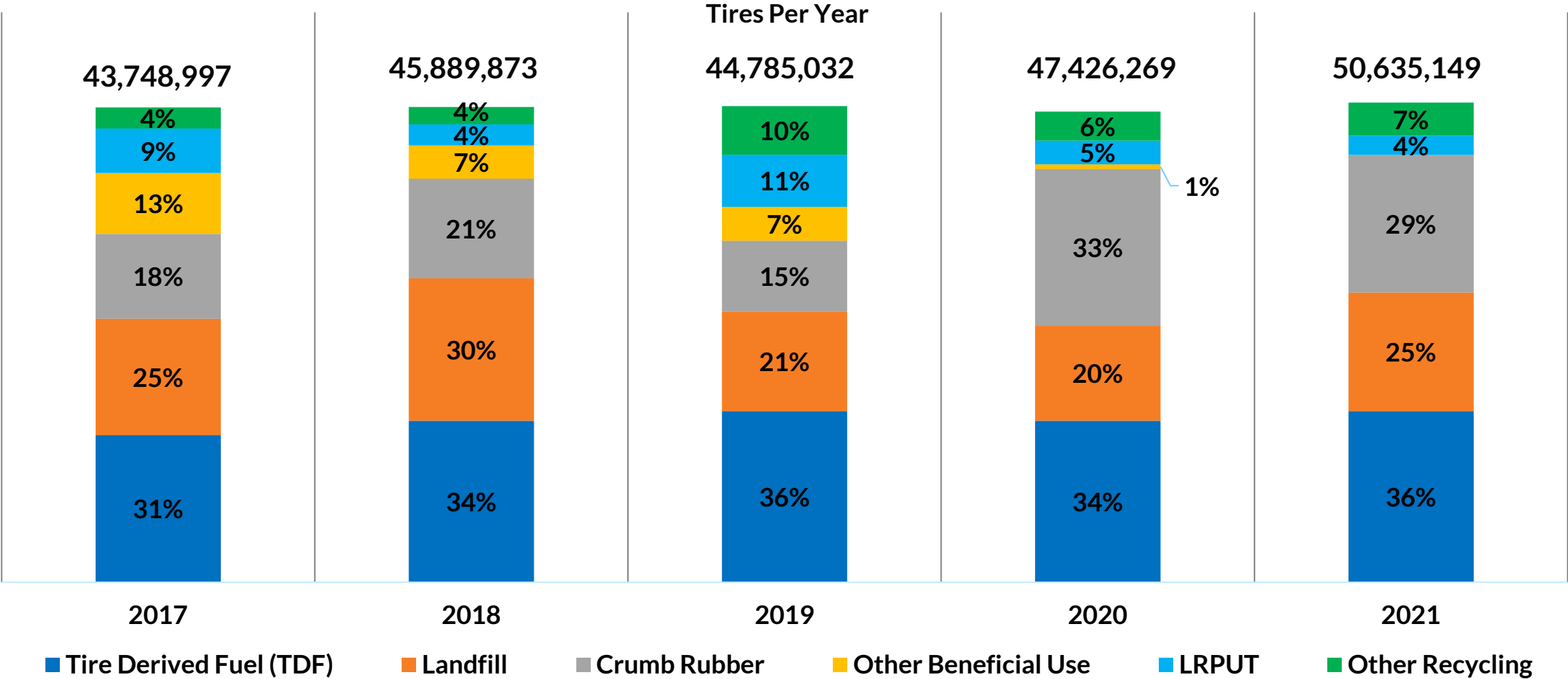
Proper End-of-Life Tire Cycle

Scrap Tire Management Stream from Generator to End-Use Facility

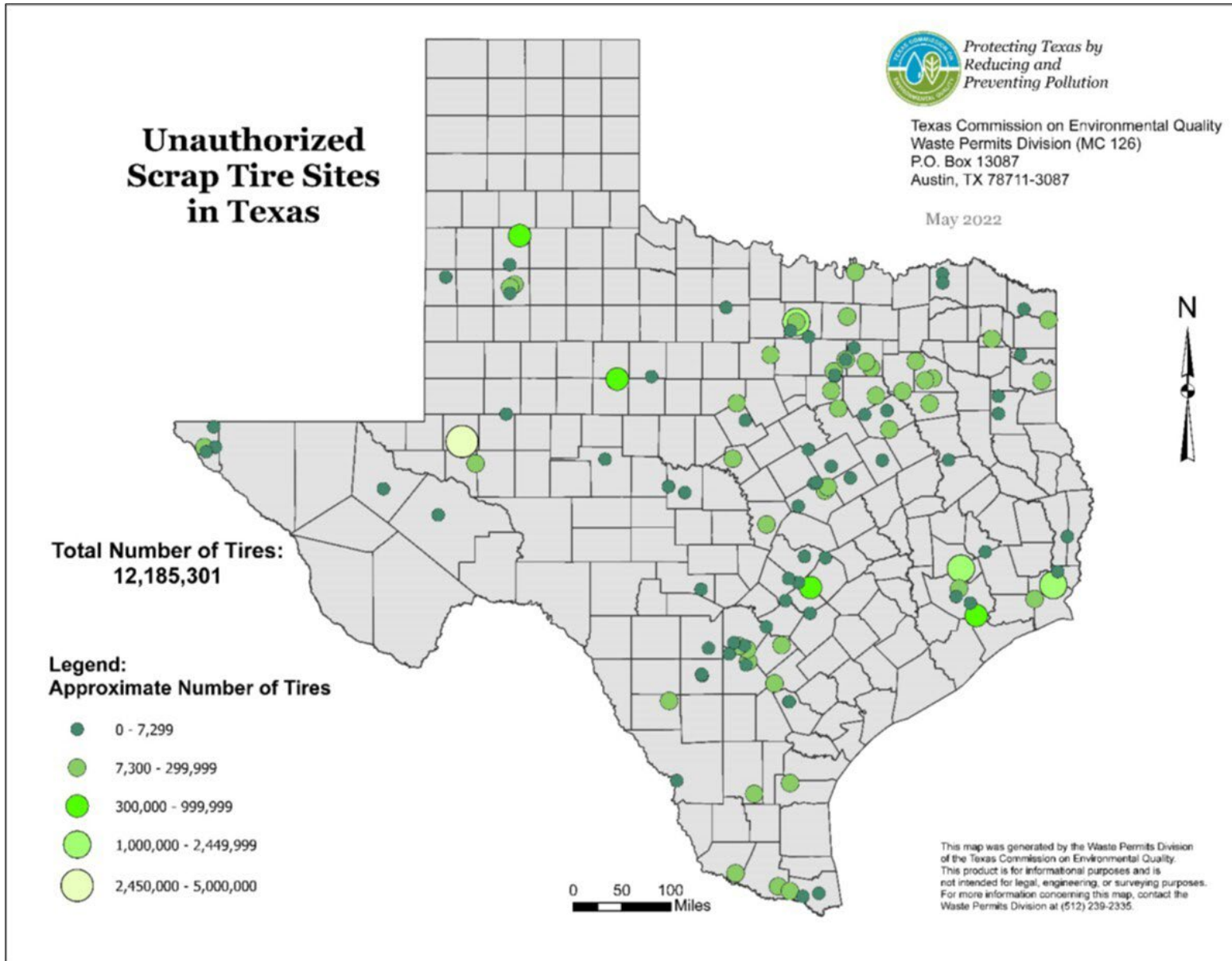


Statewide Historical Trends

END-USES FOR SCRAP TIRES 2017 TO 2021



Known Illegally Dumped Tire Sites in Texas



Total Number of Illegally Dumped Tires Known

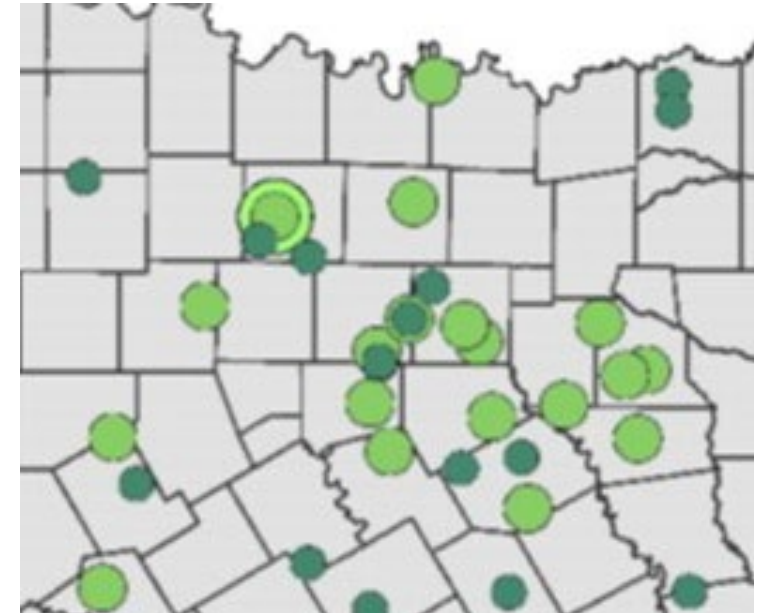
May 2022 – 12,185,301

April 2021 – 11,586,130

April 2020 – 11,751,174

April 2019 – 14,708,535

April 2018 – 14,359,826



M&O, AQ, and Regional Safety Program

Category	RTC Share
Planning Activities, Data Collection, and Engineering Activities	\$25.24M
Air Quality Initiatives	\$11.65M
Regional Tire Program	*\$490,000
Implementation/Operations Activities (e.g., Auto Occupancy, Special Events, Mobility Assistance Patrol)	\$51.74M
Travel Demand Management/Transit	\$6.75M
Innovative Technologies	\$10.40M
2023-2026 Funding	\$105.78M*

*\$490,000 committed from the Air Quality Initiatives funding recently approved by the Regional Transportation Council (RTC) for use in a regional tire program

Regional Tire Task Force

- Created December 2021
- Inter-department partnership and collaboration
- Strategy development and implementation to address regional scrap tire management in the 16-county region
- Regional Scrap Tire Model Ordinance
 - Comprehensive plan with provisions a county can adopt and edit
 - Unified model ordinance with provisions a city can adopt and edit
 - Cities with existing ordinances: Dallas, Forney, Fort Worth, Grand Prairie, Mineral Wells
 - Developed using City of Dallas updated ordinance as template and incorporating aspects of City of Houston ordinance

<https://www.nctcog.org/envir/materials-management/regional-tire-task-force>

Regional Scrap Tire Model Ordinance

Sections:

- **Definitions**
- **Tire business license and mobile tire repair unit permit required; application; transferability**
- **Issuance, denial, and display of a license permit, or tire disposal records**
- **Revocation of a license**
- **Required Training**
- **Scrap Tire Generator Storage Methods**
- **Tire Identification**
- **Transporting Scrap Tires**
- **Penalty**

Mobility Plan Bundle Policy

Sustainable Tire Recycling

One of 20 policies from the Mobility Plan

Voluntary participation

Rewards qualifying entities with TDCs

City and County Eligibility

Develop a plan, ordinance, or policy for transportation refuse recycling, or implement elements of a transportation refuse recycling plan.

3 points maximum



Current Proposed Tire Legislation

- **HB 3282** – Pending, in House Environmental Regulations Committee 4/13
 - Creates additional requirements for tire transporters and generators, creates fines for not being registered, fines go into an account to recycle tires.
- **HB 3352** – Pending, in House Environmental Regulations Committee 4/13
 - Creates programs for unique scrap tire identification and enforcement for certain counties
- **HB 1503** – Approved in committee April 4th, awaiting House debate
 - Tire disposal grant program
- **SB 1328** – No action
 - Add illegal storage of scrap tires to the current list of “common nuisances” in current statute and proposes to reduce number of legally stored scrap tires from 500 to 200
- **SB 2126** – No action
 - Authorize a county to appropriate and spend money on a program to buy back used tires, as part of managing a current solid waste program

FOR MORE INFORMATION

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<https://www.nctcog.org/envir/materials-management/regional-tire-task-force>



NCTCOG PRESENTATION

WALK TO SCHOOL DAY PROMOTION 2023

SHAWN CONRAD | STTC | 4.28.23



Nichols Elementary: 2022 1st Place Photo Contest Winner

WHAT IS WALK TO SCHOOL DAY?

- Annual event that involves schools and communities across the country and around the world walking to school on the same day
- Highlights the importance of pedestrian and bicycle safety
- Walk to School Day 2023 will be held on Wednesday, October 4
- NCTCOG helped schools participate with safety-themed giveaways in 2019 and 2022



Tadlock Elementary (left), AV Cato Elementary (top right), Taylor Elementary (bottom right)



BY THE NUMBERS – 2022



104 events were registered with the National Walk to School Day event tracker from Collin, Dallas, Denton, Ellis, Hunt, and Tarrant Counties

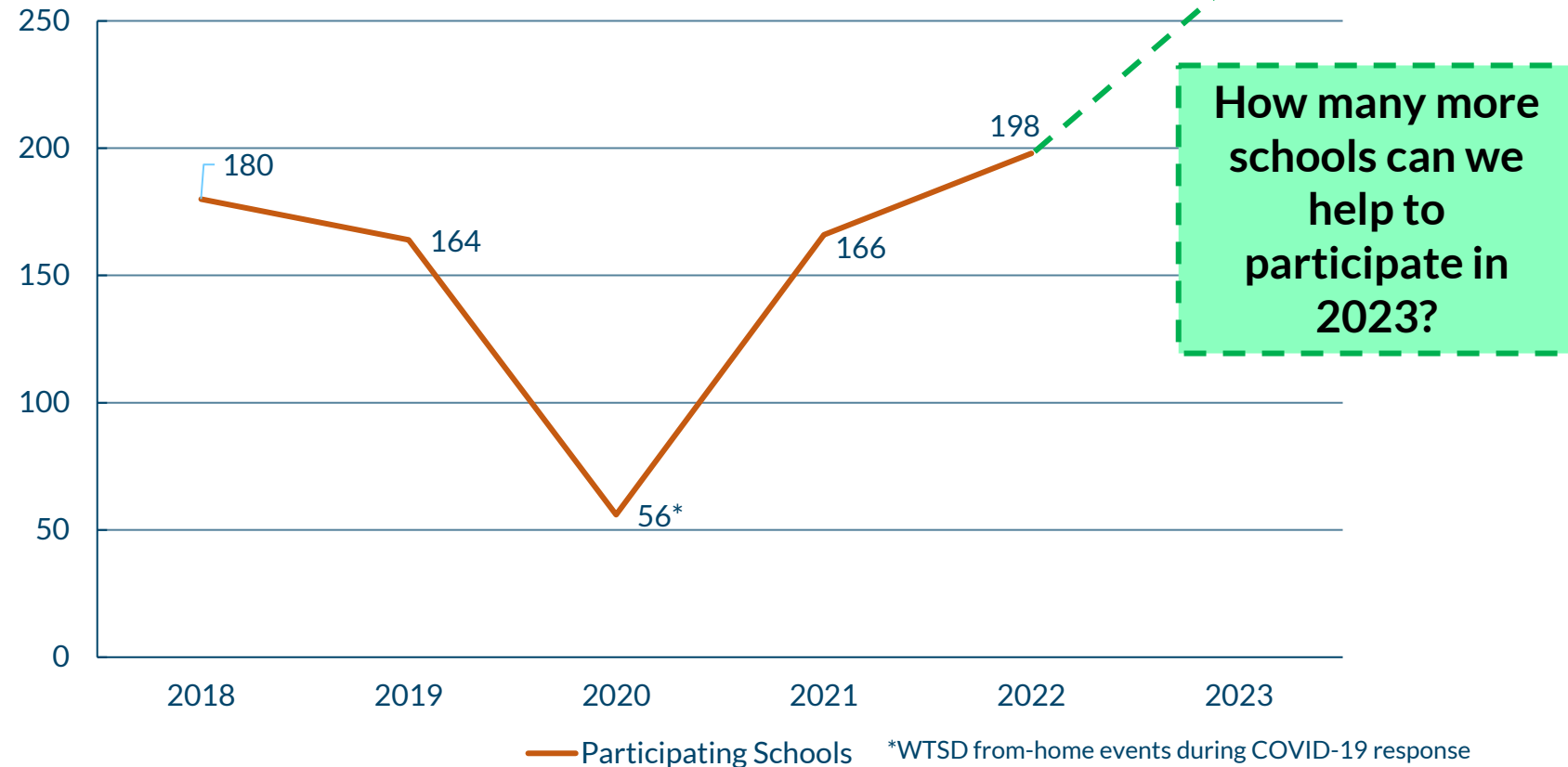


NCTCOG helped 20 schools celebrate Walk to School Day with safety-themed giveaways



NCTCOG received 973 unique entries into a bike raffle from 14 schools

Participating Schools in Texas by Year



2023 WALK TO SCHOOL DAY PROMOTION

Giveaways and Raffle for Participating Students!

- Giveaways will be awarded to schools that sign up to participate while supplies last
 - Walk to School Day banner to display during event
 - Bicycle and pedestrian safety-focused giveaways
 - Bike raffle entries for participating students
- **We are expanding on last year's program!**



Nichols Elementary

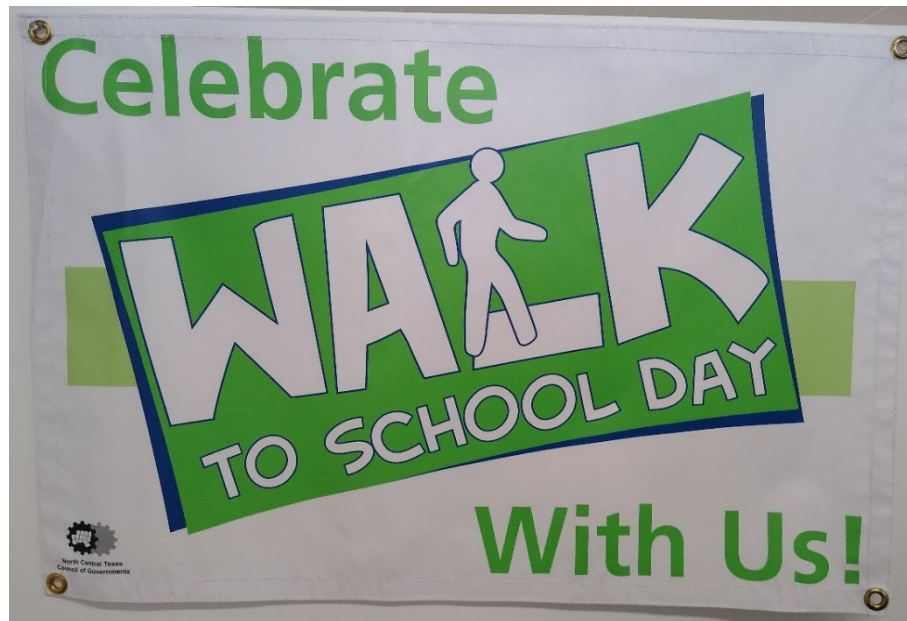


Pink Elementary



BICYCLE AND PEDESTRIAN SAFETY-FOCUSED GIVEAWAYS

- Generate excitement from students and families to participate in WTSD!
- 2022 items shown; 2023 items TBA



FRISCO – CITY/ISD COORDINATION

- City of Frisco Transportation Engineering Division
- Frisco Police Department
- Frisco Fire Department
- Frisco Independent School District



PROCLAMATION

WHEREAS, National Walk 'n Roll to School Day promotes pedestrian safety and the importance of daily physical activity for children's health and wellbeing; and

WHEREAS, a lack of physical activity plays a leading role in rising rates of obesity, diabetes, and other health problems among children. Walking to school provides an opportunity to build activity into a daily routine because active children learn better; and

WHEREAS, walking to school reduces traffic congestion, vehicle emissions, fuel consumption and encourages a cleaner and greener Frisco; and

WHEREAS, celebrating the 26th Annual National Walk 'n Roll to School Day creates awareness of the need for walkable communities and making our community a safe place for pedestrians; and

WHEREAS, this day allows for children, parents, community leaders, school representatives, and neighborhoods to come together and build a sense of community, to inspire families to walk 'n roll to school more often, and to practice good pedestrian habits which are important life skills; and

WHEREAS, the City of Frisco's Police Department, Fire Department, Transportation Engineering Division, and Frisco Independent School District partnered together with the National Center for Safe Routes to School and the Vision Zero for Youth initiative to promote Walk 'n Roll to School Day in Frisco; and

NOW THEREFORE, I, Jeff Cheney, Mayor of the City of Frisco, Texas, do hereby proclaim October 12th, 2022, as "National Walk 'n Roll to School Day" in Frisco and encourage everyone to consider pedestrian safety today and every day.

Jeff Cheney
Mayor

Proclamation read by Mayor at
Oct 4 City Council Meeting
Courtesy of the City of Frisco

- Partnership to raise awareness of pedestrian safety and celebrate benefits of walking and biking
- Registered schools had an SRO onsite on October 12 to hand out stickers
- City of Frisco staff created a toolkit to promote Walk to School Day and published materials on city website: www.FriscoTexas.gov/SchoolZoneSafety
- Participation up from 21 schools in 2021 (year one) to 30 in 2022
- FISD promoted event to individual school PTAs



NOT SURE WHERE TO START?

- Visit www.WalkBiketoSchool.com for more information about Walk to School Day and how-to guides for starting your own event!
- **Municipalities:** Collaborate with local ISDs and schools in your jurisdiction to hold joint events and raise community awareness
- **Contact us** for assistance in planning your Walk to School Day events

Bledsoe Elementary
Courtesy of the City of Frisco



PARTICIPATE WITH US!

- To be added to the Walk to School Day 2023 mailing list, fill out the interest form at www.nctcog.org/WalkToSchool
- Be the first to know when sign-ups open!

Interest Form QR Code:



Comanche Springs Elementary



CONTACT US



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Air Quality Ozone: Federal vs. State

Surface Transportation Technical Committee

April 28, 2023

Jenny Narvaez



Emissions Monitoring

Criteria Air Pollutants:

	<u>Attainment</u>	<u>Nonattainment</u>
Ozone		
Particulate Matter		
Lead		
Nitrogen Dioxide		
Carbon Monoxide		
Sulfur Dioxide		

Monitoring and Increased Regulations

Greenhouse Gas Emissions (i.e., Carbon Dioxide)



National Air Quality Standard Review/Compliance Process

Federal Clean Air Act:
EPA required to review
the NAAQS every five
years which may or may
not result in revision

**EPA Reviews and Revises
NAAQS**

State Designation Recommendations

**EPA Nonattainment Designation
and Classifications**

**Emissions Inventory
SIP Revisions**



**EPA
Reclassification**

**Area Fails to
Attain Standard**

**Area Attains
Standard**

**Redesignation Request and
Maintenance Plan SIP Revision**

Redesignation to Attainment

 **Environmental Protection
Agency (EPA) Action**
 **Texas Commission on
Environmental Quality
(TCEQ) Action**



Federal: National Air Quality Standard Review

Federal Clean Air Act:
EPA required to
review the NAAQS
every five years
which may or may not
result in revision



**EPA Reviews
NAAQS**

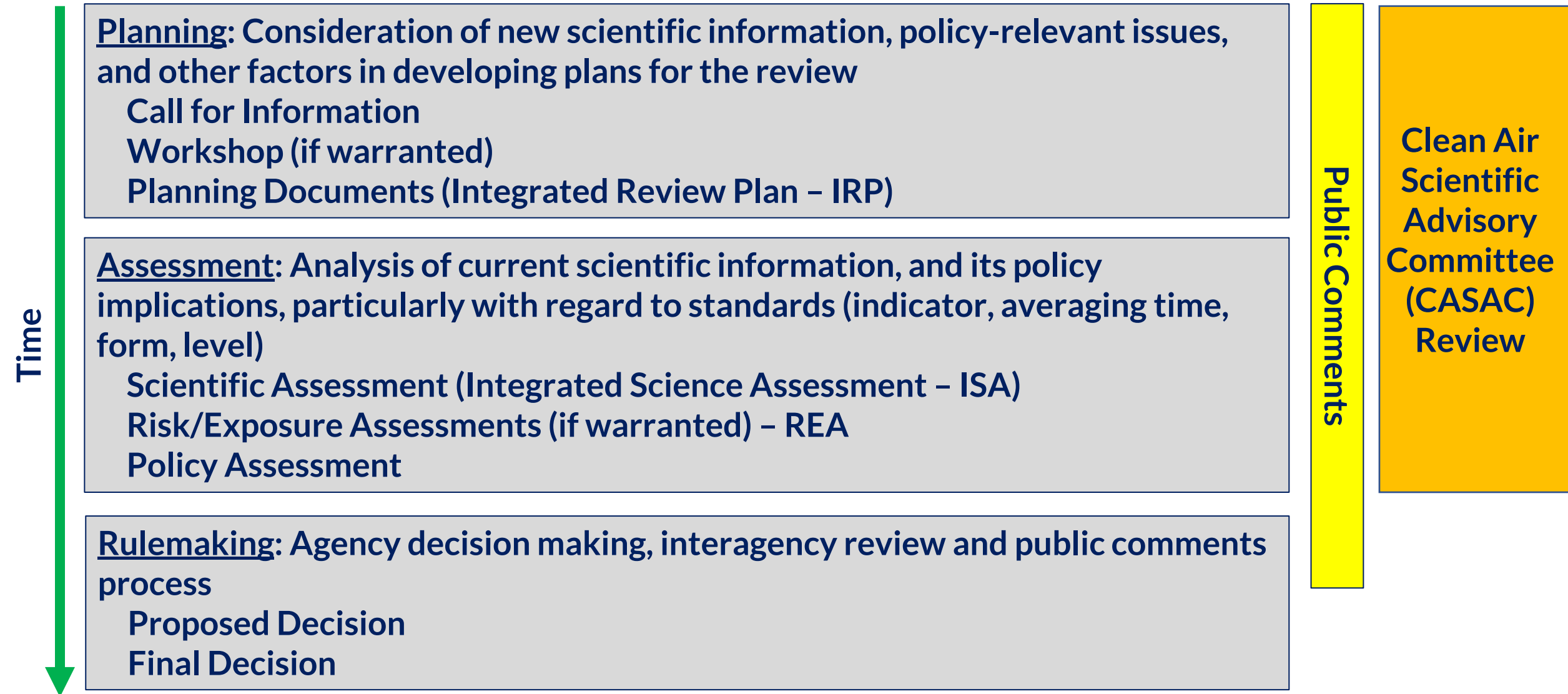


Clean Air Scientific Advisory Committee (CASAC)

- Independent scientific review committee
- Composed of seven members including:
 - Member of National Academy of Sciences
 - Physician
 - State Air Pollution Control Agency Representative
- Review assessments and make recommendation to EPA Administrator



Federal: National Air Quality Standard Review Process Overview



Federal: EPA Revises NAAQS: Assessments

Integrated Science Assessment (ISA)

Prepared by EPA's Center for Public Health and Environmental Assessment (CPHEA)

Identifies, critically evaluates and synthesizes the most policy-relevant current scientific literature (epidemiology, controlled human exposure, animal toxicology, atmospheric science, exposure science, environmental science, and ecology)

Provides focused assessment of the scientific evidence to address specific scientific questions and inform the overall policy-relevant questions

Scientific foundation for each national standard review

Risk/Exposure Assessment (REA)

Develops quantitative characterizations of exposures and associated risks to human health or the environment associated with recent air quality conditions and with air quality based on conclusion from ISA

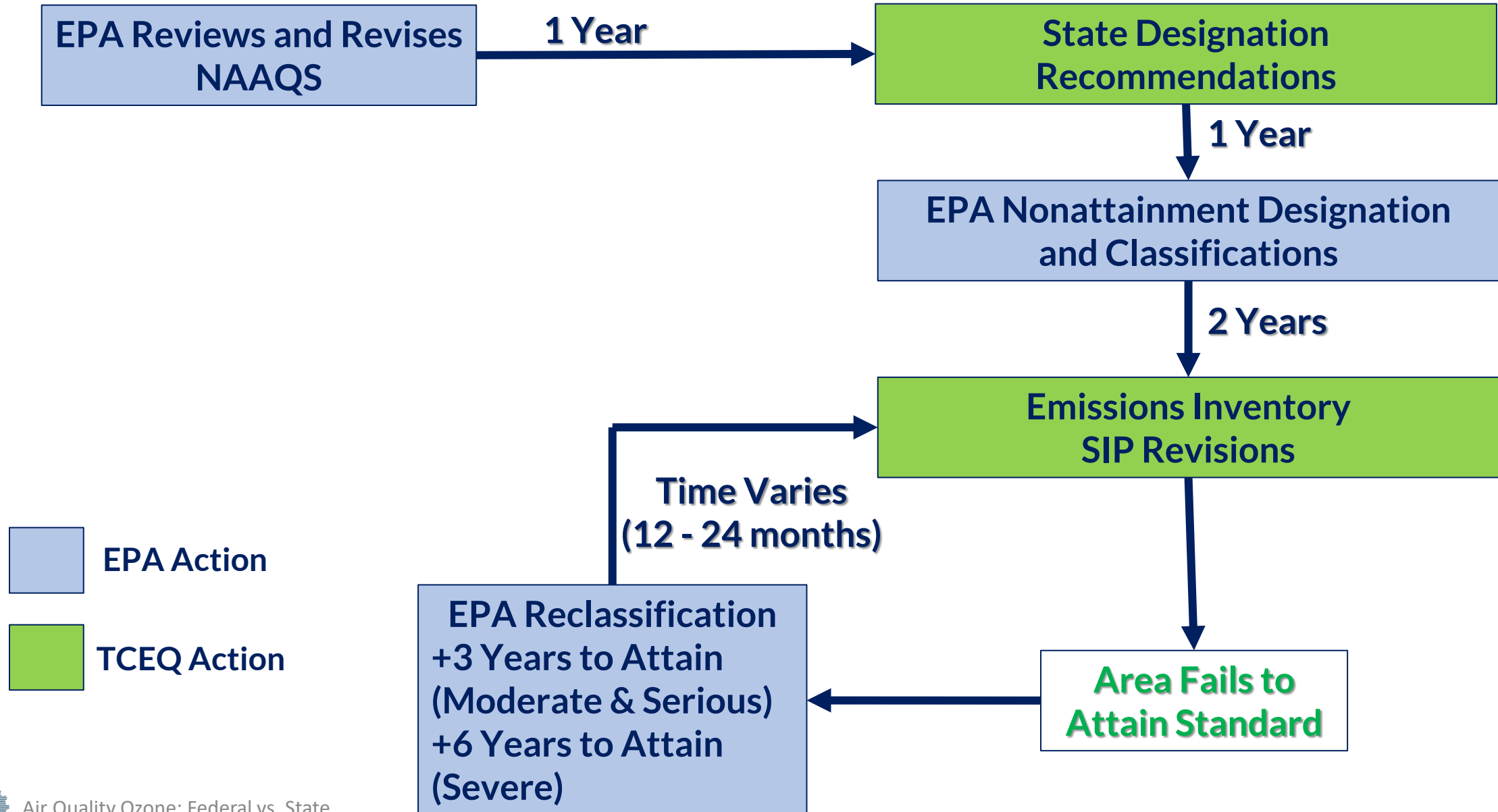
Policy Assessment (PA)

Provides an evaluation of the currently available information with regard to the adequacy of the current standards and potential alternatives

Integrates and interprets the current scientific evidence from the ISA and REA to frame policy options for consideration by the Administrator



National Air Quality Standard Review/Compliance Process



State: State Implementation Plans

State Implementation Plan Revisions – shows how a nonattainment area will improve air quality and meet the NAAQS by the attainment deadline

Reasonable Further Progress

Attainment Demonstration

Air Emissions Monitoring Data

Emissions Inventory

Photochemical Modeling

Control Strategy

Failure to submit or implement a SIP or submission of a SIP that is unacceptable to the EPA can result in:

Start of a Federal Implementation Plan (FIP) clock

SIP call

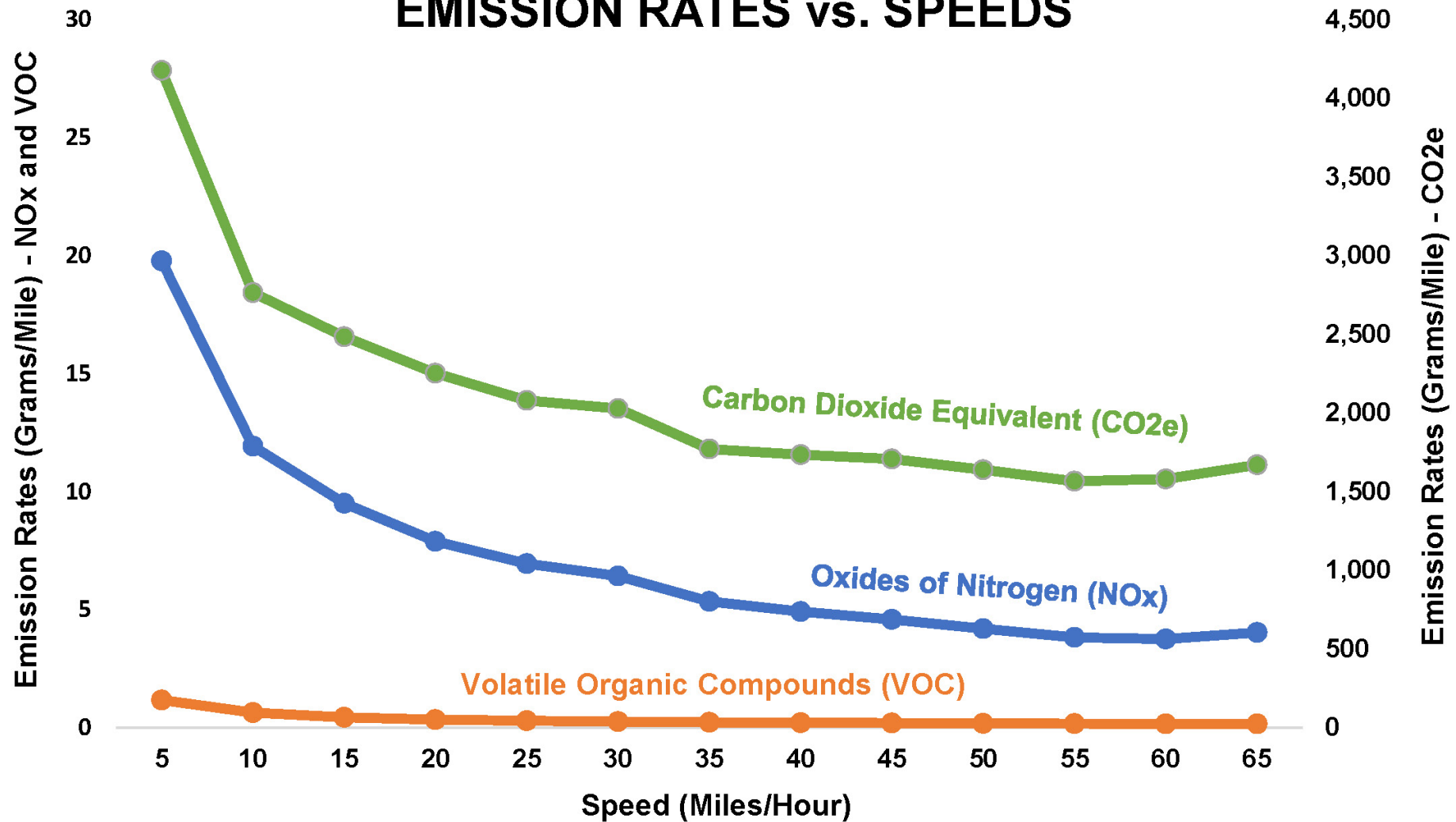
EPA sanctions clock

Sanctions can include cutting off federal highway funds and setting more stringent pollution offsets for certain emitters



Emissions Monitoring

2019 HEAVY-DUTY DIESEL TRUCK EMISSION RATES vs. SPEEDS



Topics of Concern from the RTC to the TCEQ – 04/12/2023

Major Source Emissions (Section 185) Fees

Photochemical Ozone Forecasting

State Implementation Plan (SIP) Boundaries

High Ozone Background Levels from External Influences

Tracking Upwind Emission Sources

Limited Ratios: Oxides of Nitrogen (NO_x) and Volatile Organic Compounds (VOC)

Impacts During COVID-19 Emissions Study

Vehicle Clean Scanning and Impacts to Air Quality



References

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