

### WHAT IS FUNCTIONAL CLASSIFICATION?

Functional Classification categorizes roadways by their relative purpose, typically comparing importance of traffic mobility to property access.

Federal Functional Classification System (FFCS) Types:

Interstate

Other Freeways and Expressways

Principal Arterial

Minor Arterial

Major Collector

Minor Collector

Local



## PROPOSED FFCS AMENDMENTS

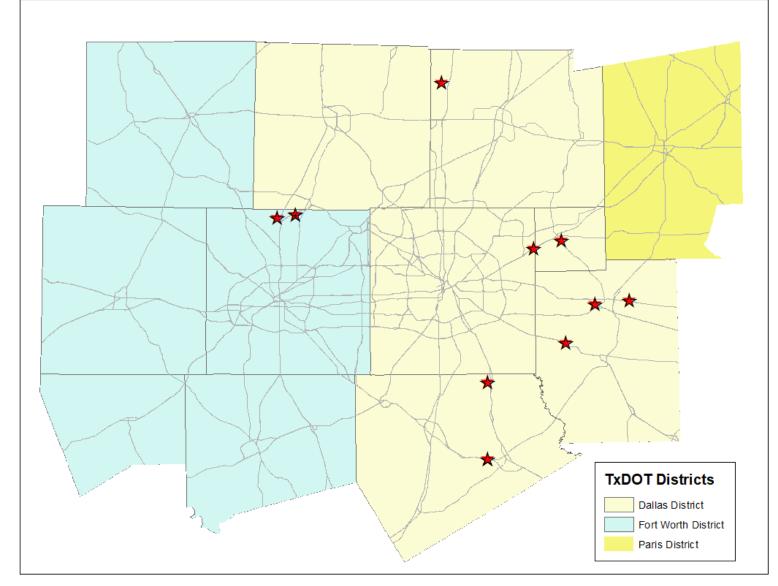
Occasionally projects in the Transportation Improvement Plan (TIP) or the Statewide Transportation Improvement Plan (STIP) are not on the Federal Functional Classification System.

Projects partially or fully funded from federal sources are required to be classified as a Major Collector or above.

Out-of-cycle updates to the FFCS require an amendment.

## PROPOSED AMENDMENT LOCATIONS

Individual project details provided in Reference Item 2.2



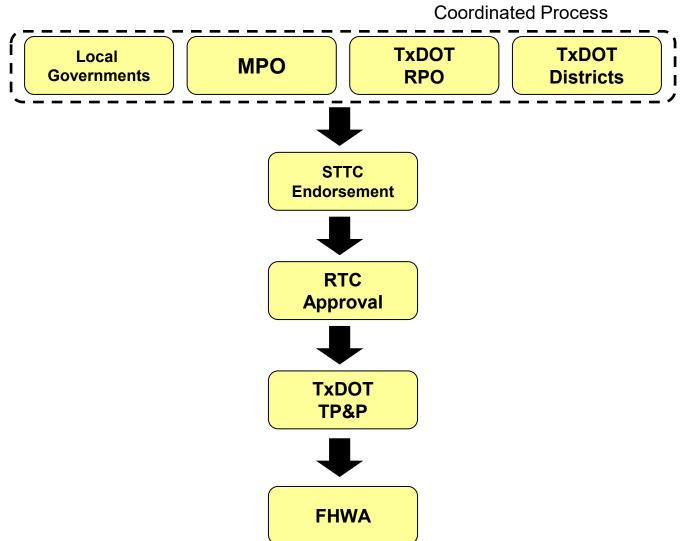
## REQUEST FOR ENDORSEMENT

Request STTC endorsement of these 10 amendments to the Federal Functional Classification System and recommendation for Regional Transportation Council approval at its July 13, 2023 meeting.





## AMENDMENT PROCESS





# Collin County Rides Program: Background

Who: Cities of Allen, Fairview and Wylie

What: Transit service for seniors and persons with disabilities

Current Provider: Dallas Area Rapid Transit (DART); transit service was previously provided

by Texoma Area Paratransit System (TAPS)

**Critical Timing:** DART contract expires September 30, 2023; initial contract expired

September 2022, but DART Board granted one year extension while cities

looked for other options

Risk: Threat for suspension of transit service leaving gaps in coverage, etc.



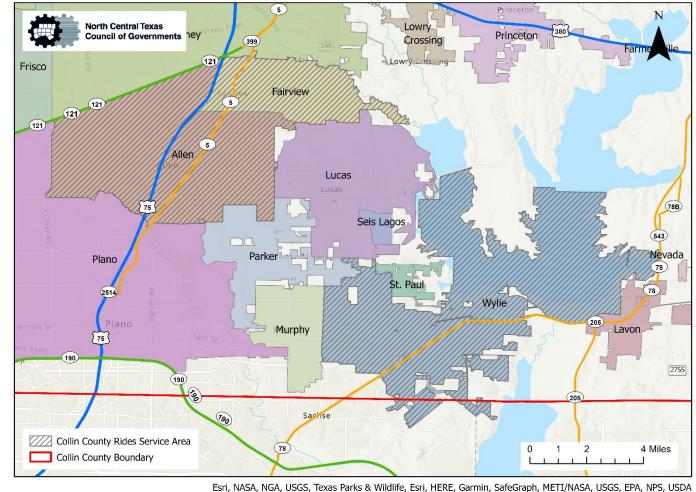
Collin County Rides Program: Service Area

Allen, Fairview & Wylie

Seniors and Persons with Disabilities

Travel anywhere in county with origin or destination in resident city

### Collin County Rides Service Area

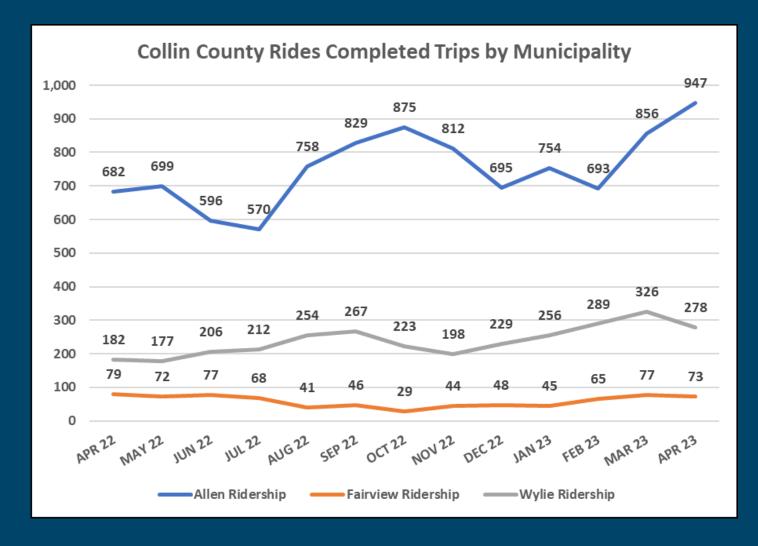






## Collin County Rides Program: Service Ridership\*

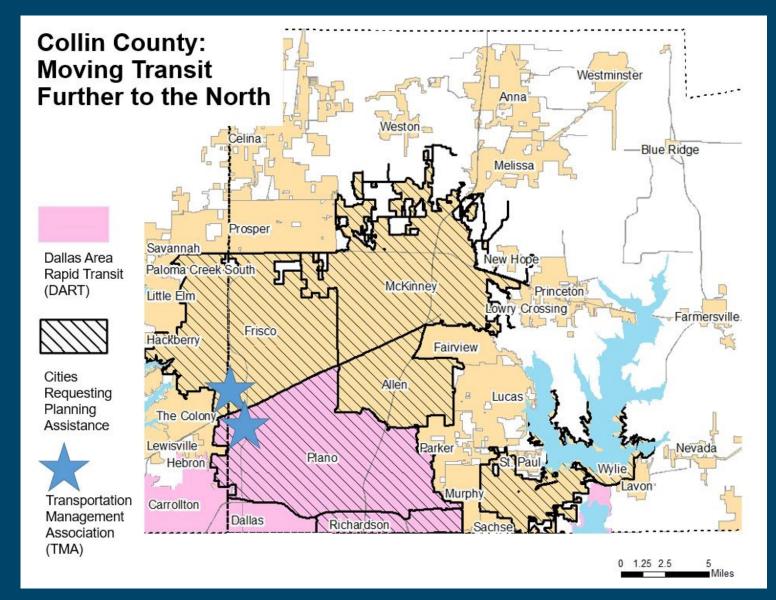
In April 2023, there were 426 registered users and 57 average weekday completed trips





## Collin County Transit Study

- Internal and Regional Connections
- Increased Transportation Options
- Funding Options
- Near-Term, Strategic Implementation
- Private Sector Involvement





# Collin County Rides Program: Proposed Funding & Service Continuation

**How Much:** 

Phased Funding	RTC Local	FTA 5310 <sup>1</sup>	Local Match <sup>2</sup>	Total
Phase I	\$300,000		\$300,000	\$600,000
Phase II		\$600,000	\$600,000	\$1,200,000
TOTAL	\$300,000	\$600,000	\$900,000	\$1,800,000

What: Continued transit service for the cities of Allen, Fairview and Wylie

When: Service start-date October 1, 2023

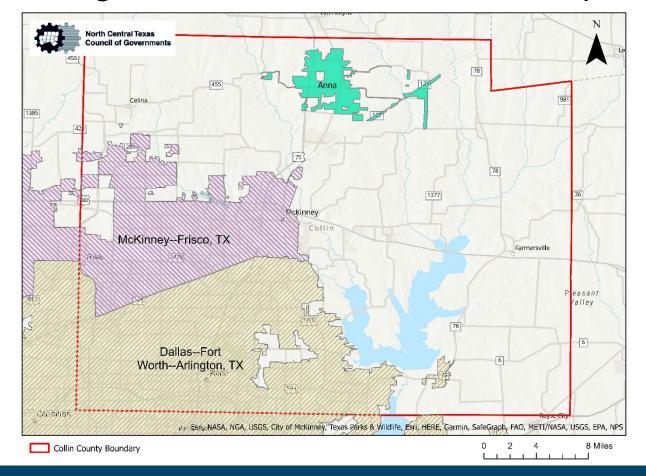
**Future Plans:** Evaluate service and possibly incorporate into existing transit authority service area



# City of Anna Transit Request: Background

- Anna residents voiced concerns over lack of affordable transit options
  - Private transportation is a financial hardship for seniors and disabled residents
  - Primary transit need is accessing medical providers in Anna and throughout Collin County
  - Some need mobility assistance for grocery shopping, pharmacy and social trips
- NCTCOG staff have been coordinating with City of Anna to explore transit funding options

#### Large Urbanized Areas in Collin County





## Transit Challenges in Collin County

- Until 2015, TAPS was the provider for Collin County. Once service was discontinued, patchwork of services was implemented to resolve transit.
  - Within urbanized areas, cities have been able to leverage federal and State funding for senior transportation.
  - Rural areas have mostly been left unserved. A Rural Transit District (RTD) will need to be established by the Collin County Commissioner's Court to access federal rural funding through the Texas Department of Transportation (TxDOT).
- The City of Anna falls under the "rural" classification by the Federal Transit Administration (FTA) because the population is under 50,000



# City of Anna Transit Request: Proposed Funding & Service Overview

**How Much:** 

Funding	Federal <sup>1</sup>	Local Match <sup>2</sup>	Total	
TOTAL	\$600,000	\$600,000	\$1,200,000	

What: Implement transit service for the City of Anna

When: Summer 2024

**Future Plans:** Work with Collin County and municipalities to establish a Rural Transit

District for the county and identify a rural transit provider so rural

communities can access FTA funds through TxDOT



## Action Requested

#### **STTC Recommendation for RTC:**

Collin County Rides Program: to utilize up to \$1,800,000 total funds (RTC Local: \$300,000; FTA Section 5310 funds: \$600,000; Local Match: \$900,000) to continue transit service in the cities of Allen, Fairview and Wylie

<u>City of Anna:</u> to utilize up to \$1,200,000 total funds (CMAQ or Carbon Reduction Program funds: \$600,000; Local Match: \$600,000) to implement transit service in the City of Anna

To revise administrative documents as appropriate to incorporate these projects



## Contact Information

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# Previously Approved ATS Funding Ongoing Projects

Pilot Projects

Study

RTC Approval Date	Project	Funding
November 2020	Dallas International District	\$10M Total \$2M Engineering \$8M Construction Source: STBG with matching TDCs
November 2020	Arlington General Motors Plant	\$11M Total \$2M Engineering \$1M ROW \$8M Construction Source: STBG with matching TDCs
October 2022	Plano/Legacy Area ATS Study	\$375K Feasibility Study Source: STBG with matching TDCs





# ATS Pilot Projects



#### **Dallas International District**

- People Movement
- Circulator in dense, mixed-use development (formerly Midtown)
- Dallas Midtown ATS & Shared Parking Feasibility Study (2019) concept
- Coordinating with City of Dallas to identify pilot segment
- Next step: pilot segment engineering design

#### **General Motors**

- Cargo/Goods Movement
- Connecting Arlington GM Plant to Union Pacific railhead over SH 360
- Test track for ATS vehicles and wireless charging technology
- Coordinating with GM and City of Arlington to identify location of bridge
- Next step: TxDOT to design bridge over SH 360





## Las Colinas ATS Modernization Pilot

#### Fall 2020

APT service suspension

DCURD requested NCTCOG review ability to retrofit guideway

#### March 2023

DCURD accepted NCTCOG's proposal to modernize a pilot segment



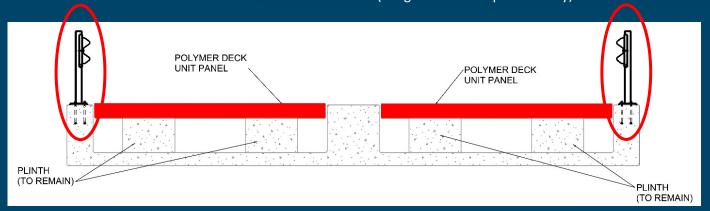
Conceptual rendering of modernized Las Colinas guideway for ATS Pilot (Image: ATS Development Study)

#### **Modernization Pilot:**

Engineering design followed by "Go /No go" milestone

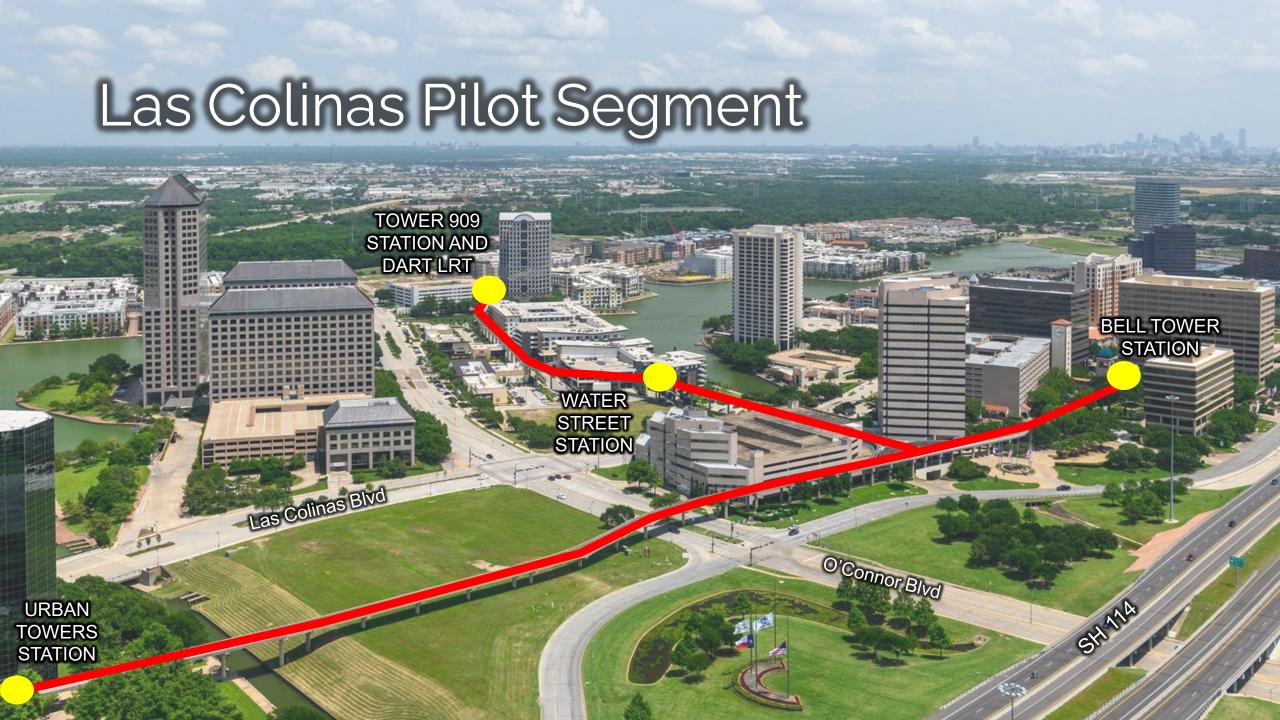
Procure ATS vehicles or "Mobility as a Service"

Operate pilot in phases for specified length of time



Modernization of Las Colinas Bidirectional Deck System (Graphic: ATS Development Study)





# Proposed Action

Туре	Purpose	Funding
Engineering/Design	Funding for <b>engineering design services</b> to modernize a pilot segment of the existing Las Colinas guideway infrastructure	\$500,000 STBG <sup>1</sup>
Staff Time	NCTCOG staff time to support advancement of Las Colinas modernization pilot, International District ATS (People), GM/Arlington ATS (Goods), and general ATS planning, including procurement/management of consultant services, coordination with project stakeholders, and other project development activities for FY2025 – FY2026	\$500,000 STBG <sup>1</sup>

#### Request RTC approval of:

- Proposed funding for engineering/design of Las Colinas APT and staff time
- Amending the Transportation Improvement Program (TIP)/Statewide Transportation
   Improvement Program (STIP) and updating any administrative and/or planning documents
   as needed to incorporate the project funding

<sup>1</sup>STBG = Surface Transportation Block Grant; To be matched with regional Transportation Development Credits





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# PM2 CONDITION TARGETS FOR NATIONAL HIGHWAY SYSTEM (NHS) PAVEMENT & BRIDGE ASSETS



JEFFREY C. NEAL - SENIOR PROGRAM MANAGER
SURFACE TRANSPORTATION TECHNICAL COMMITTEE (STTC)

ACTION ITEM - June 23, 2023

#### **NCTCOG Federal Performance Target-Setting Activities**

#### **Context - Regional Strategy**

#### Implement required Federal measures

- Fixing America's Surface Transportation (FAST) Act / Bipartisan Infrastructure Law (BIL)
- National Performance State of Good Repair
  - Ensure safety of traveling public
  - Ensure effective long-term operation of transportation infrastructure using asset management
- National Performance National Highway System (NHS) Good / Poor Condition Tracking

#### Support TxDOT targets as much as possible

- State Performance
- Set goals to guide regional planning documents & project outcomes
  - Regional Performance "Tell Our Story"
  - Include in Metropolitan Transportation Plan (MTP)
  - Include in Transportation Improvement Program (TIP)

## NCTCOG Federal Performance Target-Setting Activities (cont.)

#### Recent/Upcoming Committee Actions - PM2 (Pavement/Bridge) Targets

Rulemaking	Number of	RTC Target-S	etting Action	Reporting			
Rutemaking	Measures Recent/Upcoming		Next (Anticipated)	Period	Schedule		
Transit Safety (PTASP)	7	May 2021	Early 2025	Annually	Annually		
Transit Asset Management (TAM)	4	September 2022	Late 2026	Annually	Annually		
PM3 – System Performance, Freight, & CMAQ	7	September 2022	Late 2024	Four-Year Performance Periods (Second Period: 2023-26)	Biennially (beginning, middle, & end of performance periods)		
PM1 – Roadway Safety	5	February 2023	Early 2024 (Information/Update)	Annually	Annually		
PM2 - Pavement/Bridge Condition	6	July 2023	Late 2024	Four-Year Performance Periods (Second Period: 2023-26)	Biennially (beginning, middle, & end of performance periods)		

2022

First PM2 performance period (2018-21) ends

Second PM2 performance period (2022-25) begins

RTC adopts PM2 targets for 2024 & 2026

2024

Mid-performance period report due October 1, 2024

RTC adjusts or reaffirms 2026 PM2 targets

2026

Second PM2 performance period (2022-25) ends

Third PM2 performance period (2026-29) begins

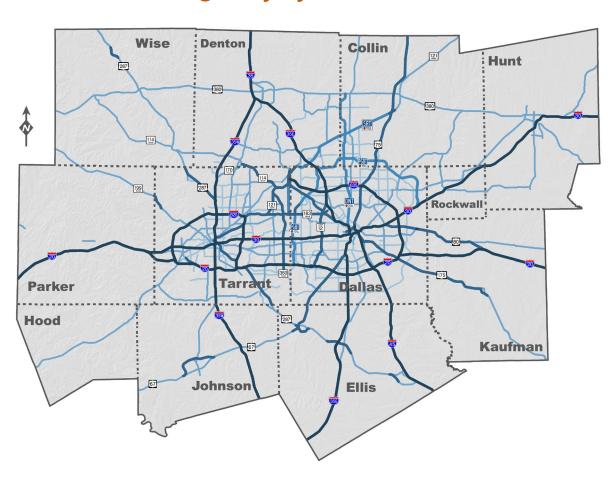
RTC adopts new PM2 targets for 2028 & 2030

## National Highway System (NHS) - NCTCOG Region

#### Review of NHS Roadway Classifications for PM2 Analysis

- State DOTs required to establish PM2 targets representing the <u>full</u> NHS extent, <u>regardless of</u> <u>ownership</u>
- Total NHS (Texas) = 70,976 lane-miles
- Total NHS (NCTCOG) = 12,448 lane-miles
  - Interstate Highways (IH) = 3,215 lane-miles (25.8%)
  - Non-IH Freeways = 1,667 lane-miles (13.4%)
  - On-System Arterials = 3,769 lane-miles (30.3%)
  - Off-System Toll Roads = 838 lane-miles (6.7%)
  - Off-System Arterials = 2,959 lane-miles (23.8%)
- NHS comprises 14% of region's total roadway lane-miles, but carries 63% of total vehiclemiles of travel (VMT)

#### **National Highway System (NHS) Facilities**



### **NHS PM2 Pavement Analysis**

#### New Pavement Data Considerations (Non-Interstate NHS Facilities)

- Pavement roughness often judged as critical link between user satisfaction (comfort/safety) and performance
  - For decades, International Roughness Index (IRI) has been a standardized & practical metric via data sensed without traffic disruption
  - Roughness & structural distress (cracking, rutting, & faulting) interdependencies do exist, but sensing limitations prevent consensus on exact analytical relationship
- Roughness (IRI) alone results in overestimation of poor condition pavements
- Non-Interstate NHS Pavements:
  - First PM2 Performance Period: IRI Only
  - Second PM2 Performance Period: IRI + Structural Distress
- Revised methodology results in positive condition differences for non-Interstate pavements
- Integration of metrics will enable more holistic lifecycle assessments of pavement assets

#### **Statewide Data/Targets - Good Condition (DRAFT)**

First Performance Period (2018-22) Second Performance Period (2022-26) **DESIRED NATIONAL HIGHWAY SYSTEM (NHS) IMPROVEMENT** 2022 2026 2024 2022 2018 2020 **ROADWAY CATEGORIES** Target 1 Baseline Target <sup>2</sup> Target <sup>2</sup> **TREND Baseline** Observed (Last Observed) (Updated 2020) (Forecast) (Forecast) State of Texas **Good Pavement Condition Interstate NHS** 66.8% 66.6% 66.5% 63.6% 64.5% 63.9% **Non-Interstate NHS** 54.4% 55.2% 54.1% 51.7% 45.5% 46.0%

- 1. Original statewide target from First Performance Period based on historic 4-year Highway Performance Monitoring System (HPMS) moving average, including transition from visual pavement surveys to semi-automated data collection; assumes International Roughness Index (IRI) ratings only for non-Interstate NHS; assumes IRI, cracking, rutting, & faulting metrics for Interstate NHS.
- 2. New statewide targets for Second Performance Period based on historic 4-year HPMS moving average, but correlated with 2022 Transportation Asset Management Plan (TAMP) 10-year lifecycle analysis projections; assumes IRI, cracking, rutting, & faulting metrics for all NHS facility pavements.



#### Regional Data/Trends - Good Condition (DRAFT)

NATIONAL HIGHWAY SYSTEM (NHS) ROADWAY CATEGORIES	DESIRED	First Perfo	ormance Peri	od (2018-22)	Second Performance Period (2022-26)			
	IMPROVEMENT TREND	2018 Baseline	2020 Observed	<b>2022 Trend <sup>1</sup></b> (Updated 2020)	<b>2022 Baseline</b> (Last Observed)	<b>2024 Trend <sup>2</sup></b> (Forecast)	<b>2026</b> <b>Trend <sup>2</sup></b> (Forecast)	
North Central Texas (NCTCOG) Region								
Good Pavement Condition								
Interstate NHS	<b>—</b>	50.1%	34.9%	19.8%	56.2%	54.5%	56.1%	
Non-Interstate NHS: On-System Freeway (TxDOT)	<b>—</b>	43.2%	48.8%	54.4%	47.8%	50.2%	56.0%	
Non-Interstate NHS: On-System Arterial (TxDOT)	<b>—</b>	36.1%	43.3%	50.9%	32.0%	<b>31.7</b> %	31.8%	
Non-Interstate NHS: Off-System Arterial (Local)	<b>—</b>	1.2%	1.1%	1.0%	1.0%	1.2%	1.4%	
Non-Interstate NHS: Off-System Toll Road (NTTA) 3		93.4%	91.4%	90.9%	89.0%	89.2%	<b>87.1</b> %	

Original trend from First Performance Period based on historic 4-year Highway Performance Monitoring System (HPMS) moving average for NCTCOG NHS pavement segments only; assumes International Roughness Index (IRI) ratings only for non-Interstate NHS; assumes IRI, cracking, rutting, & faulting metrics for Interstate NHS.

- New regional trends for Second Performance Period based on historic 4-year HPMS moving average; assumes IRI, cracking, rutting, & faulting metrics for all NHS facility pavements.
- Indicated figure/trend estimates based on NTTA's Condition Rating System (CRS) which addresses surface condition, IRI, rutting, faulting, & pavement type. NTTA CRS & TxDOT HPMS data reconciliation remains under evaluation and will not impact target action.



#### **Statewide Data/Targets - Poor Condition (DRAFT)**

Second Performance Period (2022-26) First Performance Period (2018-22) **DESIRED NATIONAL HIGHWAY SYSTEM (NHS) IMPROVEMENT** 2026 2022 2022 2024 2018 **ROADWAY CATEGORIES** 2020 Target 1 Baseline Target <sup>2</sup> Target <sup>2</sup> **TREND Baseline** Observed (Forecast) (Updated 2020) (Last Observed) (Forecast) State of Texas **Poor Pavement Condition Interstate NHS** 0.3% 0.2% 0.2% 0.1% 0.2% 0.2% 13.8% 14.2% 14.2% 1.3% 1.5% Non-Interstate NHS 1.5%

- Original statewide target from First Performance Period based on historic 4-year Highway Performance Monitoring System (HPMS) moving average, including transition from visual pavement surveys to semi-automated data collection; assumes International Roughness Index (IRI) ratings only for non-Interstate NHS; assumes IRI, cracking, rutting, & faulting metrics for Interstate NHS.
- 2. New statewide targets for Second Performance Period based on historic 4-year HPMS moving average, but correlated with 2022 Transportation Asset Management Plan (TAMP) 10-year lifecycle analysis projections; assumes IRI, cracking, rutting, & faulting metrics for all NHS facility pavements.



#### Regional Data/Trends - Poor Condition (DRAFT)

Second Performance Period (2022-26) First Performance Period (2018-22) **DESIRED NATIONAL HIGHWAY SYSTEM (NHS) IMPROVEMENT** 2022 2022 2024 2026 2018 2020 **ROADWAY CATEGORIES** Trend<sup>2</sup> Trend<sup>2</sup> Trend 1 Baseline **TREND Baseline** Observed (Updated 2020) (Last Observed) (Forecast) (Forecast)

#### North Central Texas (NCTCOG) Region

Poor Pavement Condition						
Interstate NHS	0.2%	0.7%	1.3%	0.1%	0.3%	0.2%
Non-Interstate NHS: On-System Freeway (TxDOT)	6.4%	6.8%	7.2%	0.3%	0.3%	0.4%
Non-Interstate NHS: On-System Arterial (TxDOT)	18.5%	20.4%	22.1%	0.5%	0.7%	0.8%
Non-Interstate NHS: Off-System Arterial (Local)	73.7%	74.3%	74.1%	7.6%	9.4%	11.8%
Non-Interstate NHS: Off-System Toll Road (NTTA) 3	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

- Original trend from First Performance Period based on historic 4-year Highway Performance Monitoring System (HPMS) moving average for NCTCOG NHS pavement segments only; assumes International Roughness Index (IRI) ratings only for non-Interstate NHS; assumes IRI, cracking, rutting, & faulting metrics for Interstate NHS.
- New regional trends for Second Performance Period based on historic 4-year HPMS moving average; assumes IRI, cracking, rutting, & faulting metrics for all NHS facility pavements.
- Indicated figure/trend estimates based on NTTA's Condition Rating System (CRS) which addresses surface condition, IRI, rutting, faulting, & pavement type. NTTA CRS & TxDOT HPMS data reconciliation remains under evaluation and will not impact target action.



### **NHS PM2 Bridge Analysis**

#### Statewide Data/Targets - Good/Poor Condition (DRAFT)

NATIONAL HIGHWAY SYSTEM (NHS) ROADWAY CATEGORIES	DESIRED First Performance Period (2018-22			od (2018-22)	Second Performance Period (2022-26)			
	IMPROVEMENT TREND	2018 Baseline	oline Observed Targ	<b>2022 Target <sup>2</sup></b> (Updated 2020)	<b>2022 Baseline</b> (Last Observed)	<b>2024</b> <b>Target <sup>2</sup></b> (Forecast)	2026 Target <sup>2</sup> (Forecast)	
State of Texas								
Good Bridge Condition								
All NHS Facilities <sup>1</sup>		50.7%	50.7%	50.4%	49.2%	48.5%	47.6%	
Poor Bridge Condition								
All NHS Facilities <sup>1</sup>	<b>1</b>	0.9%	1.3%	1.5%	1.1%	1.5%	1.5%	

<sup>1.</sup> All percentages based on total deck area.

<sup>2.</sup> Statewide targets for First/Second Performance Periods based on 10-year moving average.



### NHS PM2 Bridge Analysis (cont.)

#### Regional Data/Trends - Good/Poor Condition (DRAFT)

NATIONAL HIGHWAY SYSTEM (NHS) ROADWAY CATEGORIES	DESIRED	First Perfo	ormance Peri	od (2018-22)	Second Performance Period (2022-26)			
	IMPROVEMENT TREND	2018 Baseline	2020 Observed	<b>2022 Trend <sup>2</sup></b> (Updated 2020)	<b>2022 Baseline</b> (Last Observed)	<b>2024 Trend <sup>3</sup></b> (Forecast)	2026 Trend <sup>3</sup> (Forecast)	
North Central Texas (NCTCOG) Region								
Good Bridge Condition								
All NHS Facilities <sup>1</sup>	<b>—</b>	55.3%	56.0%	57.9%	49.9%	50.4%	50.2%	
Poor Bridge Condition								
All NHS Facilities <sup>1</sup>	<b>1</b>	1.9%	2.3%	2.0%	2.1%	2.1%	2.2%	

- 1. All percentages based on total deck area for regional NHS bridges only.
- 2. Estimation/reporting of regional trend for First Performance Period based on 6-year moving average; condition data reported in 2-year increments.
- 3. Estimation/reporting of new regional trend for Second Performance Period based on 10-year moving average; condition data reported in 2-year increments.



### NHS PM2 Bridge Analysis (cont.)

#### **Extent/Status of Regional Poor Condition NHS Bridges**

- In considering FY 22 Bridge Investment Program (BIP) candidates, staff coordinated with local governments and TxDOT's Bridge Division/local Districts to analyze poor condition NHS bridges
- INFRA Grant (2019) North Central Texas Strategic NHS Bridge Program:
  - Original Submittal \$229 million (\$113 million INFRA requested) for 12 projects across both East & West subregions
  - Awarded Project \$45.5 million (\$8.8 million INFRA awarded) for seven projects (3 East; 4 West)
  - Implementation \$28.5 million for four projects (1 East; 3 West)
    - All implementation projects now under construction or completed
    - All projects from original submittal have treatments underway or funded/scheduled, except for one (still on poor condition list\*)
- Poor Condition NHS Bridges (2022) Breakdown by TxDOT District:
  - Dallas: 37 bridges (36 "On-System"; 1 "Off-System")
  - Fort Worth: 12 bridges (9 "On-System"; 3 "Off-System")
- Of the 49 regional NHS bridges in poor condition, treatments for all but <u>three</u> structures are funded/scheduled for construction, under construction, or completed:
  - TxDOT (City of Dallas): Loop 12 NB bridge (to IH 35E NB) over IH 35E SB\*
  - TxDOT (City of Fort Worth): SH 121 WB/Belknap Street bridge over IH 35W SB
  - TxDOT (City of Fort Worth): SH 199 (Jacksboro Highway) bridge over West Fork Trinity River

### **PM2 Target Decision-Making**

#### **Schedule**

February 10, 2023	TxDOT Submits PM2 Targets to Federal Highway Administration (FHWA)
	(180-day MPO review begins)

May 26, 2023	STTC Fast Fact
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June 8, 2023	RTC Information

June 12, 2023	NCTCOG Transportation Public Meeting
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(comment period ends July 11, 2023)

June 23, 2023 ST	IC	Action	Π
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July 13, 2023 RTC Action

August 9, 2023 Deadline for MPOs to Report Whether They Will Either:

(i). Agree to plan/program projects contributing to affirmed TxDOT PM2 targets; or,

(ii.) Commit to new quantifiable PM2 targets for the Metropolitan Planning Area (MPA)

## PM2 Target Decision-Making (cont.)

#### **Proposed STTC Action – Affirm RTC Support for New Statewide Targets**

# Good

Affirm NCTCOG support for new TxDOT statewide "Good Condition" NHS
pavement and bridge targets for 2024 and 2026

Affirm NCTCOG support for new TxDOT statewide "Poor Condition" NHS
pavement and bridge targets for 2024 and 2026

- Collaborate to plan/program projects contributing toward accomplishment of pavement/bridge goals via the following actions:
  - NCTCOG will continue to work with local governments on annual monitoring and expediting improvements for identified NHS Off-System Arterials in "Poor Condition"
  - NCTCOG will continue to work with TxDOT/local governments on annual monitoring and expediting improvements for identified NHS Bridges in "Poor Condition"

#### **CONTACT INFORMATION**

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#### <u>EXHIBIT I-8</u> Unified Planning Work Program Policies

- The emphasis of the Unified Planning Work Program (UPWP) is on planning activities. Projects that require preliminary engineering or design services are not eligible for UPWP Transportation Planning Funds. Such projects should be funded through the Transportation Improvement Program process.
- Twenty-five percent of Federal Transit Administration Section 5303 Planning Funds will be reserved for transit planning and management studies with the funds distributed to the Eastern and Western Subregions based on the formula used for the annual sub-allocation of Transit Formula (5307) Program funds.
- ➤ The Texas Department of Transportation (TxDOT) will assist the North Central Texas Council of Governments (NCTCOG) in funding activities related to the preparation of travel forecasts to support TxDOT freeway and high-occupancy vehicle lane studies.
- The North Texas Tollway Authority (NTTA) will assist NCTCOG in funding activities related to the preparation of travel forecasts to support NTTA feasibility studies.
- The transportation authorities will assist NCTCOG in funding activities related to the preparation of travel forecasts to support rail and high-occupancy vehicle lane studies.
- Assistance in Transportation Subarea Studies, Comprehensive/Thoroughfare Planning, Local Technical Assistance, and TransCAD Support will represent approximately 20 percent of the UPWP. Priority will be given to these subtasks identified in the UPWP.
- NCTCOG assistance to local governments in Thoroughfare Planning will be limited to providing travel forecasts to determine facility sizing and lane warrants, in Comprehensive Planning to providing travel forecasts and technical support, in Bicycle/Pedestrian Planning to providing technical guidance on plan and facility development, and in Travel Model Support (TransCAD) to providing model input data and application support. Local governments will be responsible for consultant assistance funding.
- For projects not specified in the UPWP, the requesting entity may be asked to assist in project funding. Scheduling of the planning study is contingent on NCTCOG staff availability. Reprioritization of tasks for that entity using existing commitments in the UPWP is feasible. Work will not be done at the expense of other commitments in the UPWP.
- Entities with consultants requesting work from NCTCOG must follow the same policy as above. Consultants working for an entity need the approval of that entity prior to NCTCOG providing assistance. Consultants shall not commit NCTCOG services without NCTCOG's prior approval.
- > Requests by nongovernmental entities will be conducted at full cost upon approval from the impacted local government and permitting staff availability.
- In the event that a requested planning study is similar to a previous study conducted by NCTCOG, the participating entity may be asked to provide financial support for the project.
- Surface Transportation Block Grant Program funds, Congestion Mitigation and Air Quality Improvement Program funds, Regional Toll Revenue funds, and Regional Transportation Council Local funds may be used by NCTCOG to augment UPWP funds.

## FY2024 and FY2025 Unified Planning Work Program for Regional Transportation Planning

**Surface Transportation Technical Committee June 23, 2023** 

## Five Major Tasks

- **Task 1 Administration and Management**
- **Task 2 Transportation Data Development and Maintenance**
- Task 3 Short-Range Planning, Programming, and Operations
- **Task 4 Metropolitan Transportation Plan**
- Task 5 Special Studies

#### Implemented through:

- 34 Subtasks
- 175-200 Programs and Projects

#### Funded through:

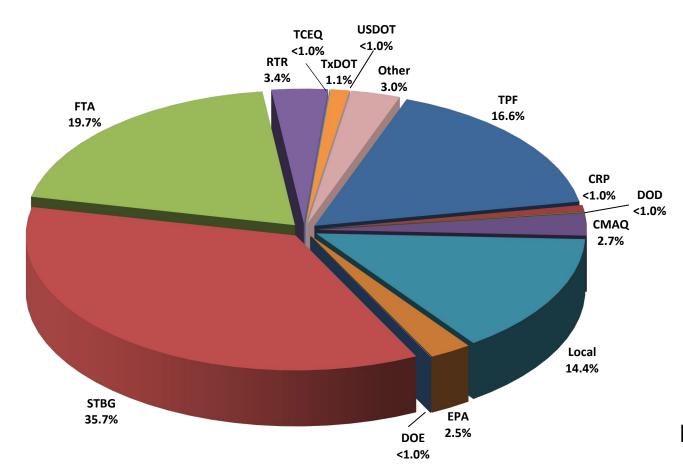
- 17 Revenue Sources
- 170 Grants

# FY2024 and FY2025 TPF Funding Summary

FY2024 and FY2025 US FTA (5303)	\$ 7,687,168
FY2024 and FY2025 US FHWA (Estimated PL)	\$20,052,092
FY2023 US FHWA (Estimated PL-Carryover)	\$ 5,524,849
Total Transportation Planning Funds	\$33,264,109
Expected Expenditures	\$28,466,880
PL Balance to Carry Over to FY2026	\$ 4,797,229

## Summary of All Funding

Estimated Total Funding (all sources): \$178,981,150



Pass-through 33%

## Development Schedule

February 9	RTC Notification of Upcoming UPWP Development
February 14	Issuance of Requests for NCTCOG Assistance to Transportation Partners, including STTC
February 15	Issuance of Requests for Regional UPWP Project Ideas to RTC
February 24	STTC Reminder for Submittal of NCTCOG Assistance Requests
March 3	Project Submittals Due for NCTCOG Assistance
<b>May 26</b>	Draft Recommendations to STTC for Information
June 5	Draft UPWP Due to TxDOT and Concurrently Provided to STTC
June 8	Draft Recommendations to RTC for Information
June 12	Initiation of Public Outreach
June 23	STTC Action on Recommended UPWP
July 13	RTC Action on Recommended UPWP
July 27	Executive Board Action on Recommended UPWP
July 31	Final UPWP Due to TxDOT

## Requested STTC Action

Recommend RTC approval of the FY2024 and FY2025 Unified Planning Work Program (UPWP), including the UPWP Policies contained in Exhibit I-8 of the Work Program.

#### And

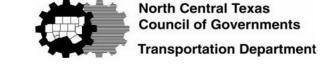
Recommend that the RTC direct staff to also administratively amend the Transportation Improvement Program and other administrative/planning documents, as appropriate, to reflect the approved projects.

## Unified Planning Work Program Contact Information

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Vercie Pruitt-Jenkins Administrative Program Coordinator 817-608-2325 VPruitt-Jenkins@nctcog.org



# SOUTH DALLAS INLAND PORT TRANSIT BRIDGE FUNDING

## SURFACE TRANSPORTATION TECHNICAL COMMITTEE JUNE 23, 2023

SHANNON STEVENSON
SENIOR PROGRAM MANAGER, TRANSIT MANAGEMENT & PLANNING

## **Background**

November 2020: Service began through a partnership between IPTMA, STAR Transit,

and DART Mobility Service LGC with funding approved by the

Regional Transportation Council (RTC).

Summer 2022: Dallas County recently approved the creation of a Local

Government Corporation (LGC) and effort are underway to move

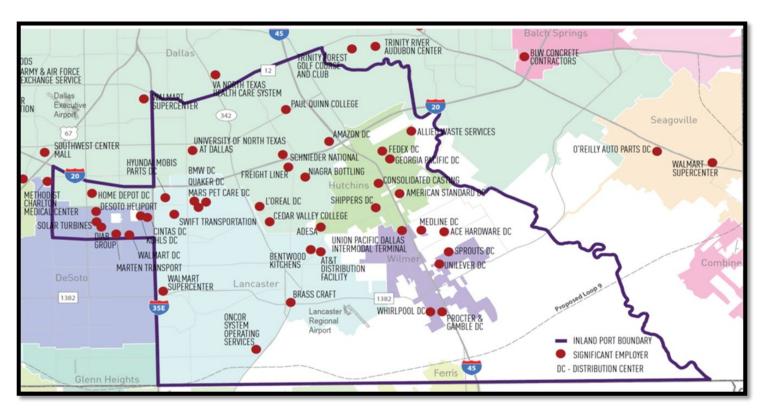
this forward.

**Spring 2023:** IPTMA requested bridge funding, which will be a one-time request,

to support increased demand while the LGC is established and the

IPTMA restructures to secure more sustainable funding.

#### **Inland Port Zone & Businesses**



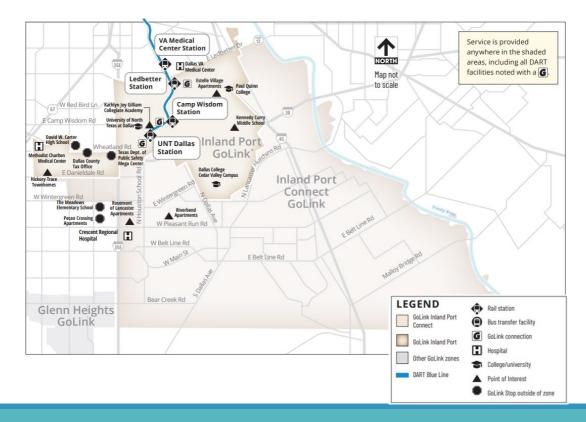
#### **Inland Port Transit Services**



GoLink Inland Port Zone, service in Southern Dallas from 5:00 am to 12:00 am, 7 days a week. Supported with MV Transit and UberPool service.



**Expanded Microtransit services** with STAR Transit and UberPool in the Cities of DeSoto, Hutchins, Lancaster, and Wilmer 5:00 am to 8:00 pm, Monday – Friday.

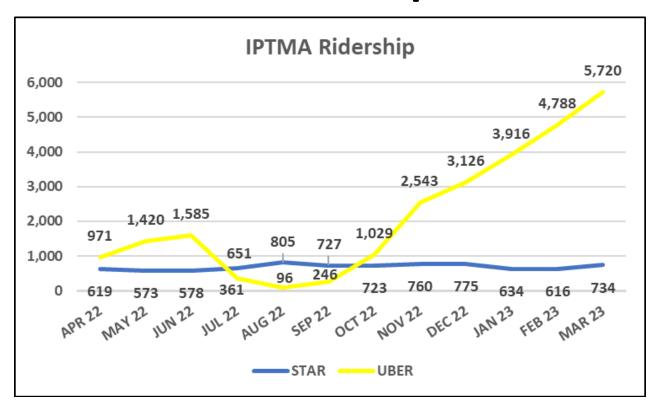


## **Inland Port TMA Ridership**

Since the UberPool integration, average weekday ridership has increased to 301 in March 2023.

The temporary decline in UberPool ridership was a result of improper geofencing.

Working to add capacity to reduce reliance on UberPool.



#### **Southern Dallas IPTMA Transit Request**

Amount: \$3,500,000 federal funds (STBG, CMAQ or Carbon Reduction

Program funding) for a two-year period (utilize 700,000 in

Transportation Development Credits in lieu of local match)

What: One-time bridge gap funding

How: Funding exchange with DART to add federal funds on the Silver

Line Project and transfer local funding for IPTMA transit services

Future Plans: The IPTMA is coordinating with partners to sustain service through

Public-Private Partnerships between cities, chambers of commerce,

transit agencies, and private employers

## **Action Requested**

#### **STTC Recommendation for RTC Approval to:**

Utilize up to \$3,500,000 in federal funds (STBG, CMAQ or Carbon Reduction Program funding) for the Silver Line Rail Project, with 700,000 in Transportation Development Credits (TDCs) in lieu of local match

DART to transfer \$3.5 million in local funding to support transit in the Southern Dallas Inland Port area

Administratively amend the Transportation Improvement Program and other planning/administrative documents as appropriate to incorporate this project

#### For More Information

#### **Shannon Stevenson**

Senior Program Manager Transit Management & Planning

> sstevenson@nctcog.org 817-608-2304

#### **Gypsy Gavia**

Principal Transportation Planner Transit Management & Planning

ggavia@nctcog.org 817-695-9134

## City of Dallas Funds from DART

	Before Mediation	Proposed <sup>1</sup>	Notes
DART Excess	\$111.1M	\$111.1M	
Betterments	-36.0 M	-5.2M	
Delays	-43.5M	-15.9M	
Balance to Dallas	31.6M	90.0M	
DART to NCTCOG		-10M	RTC June
Net Balance from DART		\$80M	
		(\$50M + \$30M)	
NCTCOG #1		5.3M	RTC July?
NCTCOG #2		15.4M	RTC June
Net-Net for Dallas		\$100.7M	



## RTC APPROVAL OF FUNDING TO SUPPORT DALLAS AND DART RESOLUTION TO ADVANCE THE SILVER LINE REGIONAL RAIL PROJECT

The RTC is requested to approve \$5.3M in federal funds for DART and the City of Dallas as part of a comprehensive resolution between the parties to advance the DART Silver Line Regional Rail Project. This funding is in addition to the \$15.4M in federal funds the RTC approved in June 2023 for the Dallas Five Mile Creek Hike and Bike Trail.

As part of this approval, the RTC requests all local governments, transportation providers, and their associated elected officials, board members, and staff to work in a coordinated and cooperative fashion to advance both the rail and trail elements of the Silver Line Regional Rail project as reflected in the regional Mobility 2045 Plan.

Should future questions or issues arise, the RTC strongly encourages all parties to work collaboratively in a professional manner to expeditiously resolve.



## FEDERAL UPDATE

#### **DEBT CEILING**

The Fiscal Responsibility Act of 2023 was approved by Congress on June 1

- Suspends the Federal government's debt ceiling through January 1, 2025
- Imposes two-year discretionary spending caps, Federal Highway Programs are exempt
- Rescinds some unobligated COVID-19 funds, transit not affected
- Streamlines environmental review processes



## FEDERAL UPDATE

#### **FAA REAUTHORIZATION**

Provides more than \$107 billion for the FAA for fiscal years 2024 through 2028.

- Last stage of NextGen completed by December 31, 2025
- FAA must stablish a pathway for beyond visual lineof-sight operations
- New safety certification of advanced air mobility powered-lift aircraft



## **TEXAS LEGISLATURE**

#### 1st SPECIAL SESSION

Governor Abbott declared a Special Session on border security and property taxes

- The House took action and adjourned on May 30
- Additional Special Session expected on school vouchers and to finalize property taxes
- Senate Finance Committee met on June 20



## STATE BUDGET

House Bill 1 – General Appropriations Act for FY24-25

Total Funding: \$321.3 billion

TxDOT Funding: \$37.2 billion

#### **Riders of interest:**

- Southern Gateway Deck Park matching funds if federal grant is awarded
- TxDOT task force to evaluate medium- and heavyduty charging infrastructure and capacity



#### **Transportation Funding**

- SB 505 Creates a \$200/\$400 annual electric vehicle fee
- SCR 2 Extends Proposition 7 expiration date to 2042 (State sales tax portion) and 2039 (motor vehicle sales tax portion)
- HB 2230 Extends Proposition 1 expiration date from 2034 to 2042

#### **General Transportation**

• **HB 3444** Texas Transportation Commission (TTC) to prescribe criteria for the classification of each district as metropolitan, urban, or rural



#### **Safety**

- HB 898 Harsher penalties for the Move Over, Slow Down Law
- HB 1885 TTC authority to establish a variable speed limit program to lower speed limits by 10 mph in certain situations

- HB 3297 Elimination of vehicle safety inspection for noncommercial vehicles
- HB 4797 Training on the treatment of tollways during inclement weather

#### **Temporary Tags**

- HB 718 Ends temporary paper tags and requires the issuance of metal plates by dealers
- HB 914 Increases penalties for temporary tag tampering
- HB 2195 Increases penalties for fictitious, altered plates

#### **Unmanned Aircraft**

• **SB 1308** Prohibits drones over airports & military installations

#### **Broadband**

- HB 9/HJR 125 Creates the Broadband Infrastructure Fund, allocates \$1.5 billion, requires voter approval
- SB 1238 Revises existing Broadband Development Program



#### **Air Quality & Electric Vehicles**

- SB 1397 TCEQ Sunset bill, continues the agency for 12 years
- SB 1732 Creates standards for EV charging stations, requiring a widely compatible connector or plug
- HB 3014 Exempts EVs from emissions inspection
- HB 4885 Creates the Hydrogen Infrastructure, Vehicle, and Equipment Grant Program under TERP

#### Water

SB 28/SJR 75 Creates the Texas Water Fund and the New Water Supply for Texas Fund, requires voter approval



## **CONTACT US**



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**Communications Coordinator** 

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NCTCOG Legislative Updates: <a href="https://www.nctcog.org/legislative">www.nctcog.org/legislative</a>

Texas Legislature Online: <a href="https://capitol.texas.gov/">https://capitol.texas.gov/</a>



# 2024 UNIFIED TRANSPORTATION PROGRAM (UTP) AND REGIONAL 10-YEAR PLAN UPDATE

Surface Transportation Technical Committee

June 23, 2023



## **BACKGROUND**

- Texas House Bill (HB) 20 requires that Metropolitan Planning Organizations (MPO) develop 10-Year Plans using performance-based planning and project selection methods.
- Includes projects funded with Category 2 (MPO selected), Category 4 (TxDOT District selected), and Category 12 (Texas Transportation Commission [TTC] selected)
- The Regional 10-Year Plan was originally approved by the Regional Transportation Council (RTC) in December 2016.
- This plan is updated annually in conjunction with the development of TxDOT's UTP.

## **ACTIVITIES SINCE LAST UPDATE**

- TxDOT began developing the 2024 UTP
- NCTCOG staff has coordinated with the TxDOT Districts regarding project updates (e.g., costs/funding, environmental clearance and let dates), and potential new projects
  - Includes 2023 projects that need additional funding to cover cost overruns at letting
- To satisfy a February 21, 2023, deadline set forth by TxDOT, a draft project listing was developed that included project revisions and potential new projects
- Feedback was received from TxDOT Headquarters regarding the region's funding requests in late May

## PRINCIPLES FOR THE DEVELOPMENT OF THE REGIONAL 10-YEAR PLAN

- Project should be included in the Metropolitan Transportation Plan
- Focus on "system" versus new, stand-alone projects
- Fully fund existing projects before funding new projects (with a focus on projects letting in the next 2-3 years)
- Ensure equity of county allocations
- Maintain toll lanes/toll managed lanes on selected corridors
- Re-fund previously unfunded projects, when possible
- Ensure all RTC projects are approved in 2024 UTP (including "placeholders")
- Projects must be scored and should have a score sufficient to qualify for funding

# REGIONAL FUNDING ALLOCATIONS FOR 2017-2024 UTPs

Funding Category*	2017 UTP	2018 UTP	2019 UTP	2020 UTP	2021 UTP	2022 UTP	2023 UTP	2024 UTP (Proposed)
Category 2	\$3.784	\$3.607	\$3.832	\$3.516	\$2.913	\$2.931	\$3.205	\$3.470
Category 4	\$0.830	\$1.553	\$1.637	\$1.537	\$1.340	\$1.348	\$1.593	\$2.341
Category 12	\$0.812	\$2.130	\$1.395	\$3.041	\$3.089	\$2.601	\$3.132	\$4.082
<b>Total Allocation</b>	\$5.426	\$7.290	\$6.864	\$8.094	\$7.342	\$6.880	\$7.930	\$9.893

<sup>\*</sup> Amounts shown in billions

## PROPOSED CATEGORY 2/ CATEGORY 12 EXCHANGE

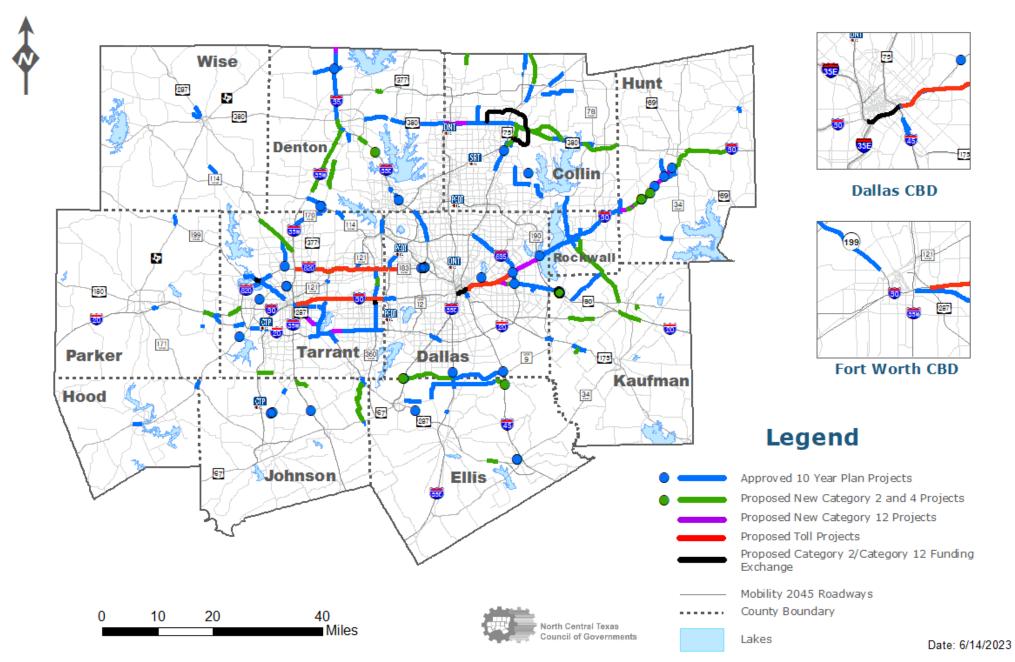
- There is currently too much Category 12 funding slotted statewide in the first few years of the UTP.
- To help resolve this issue, the region has been asked by TxDOT Headquarters to trade out Category 12 funds on the IH 30 Canyon project in 2024 and replace that funding with Category 2 and 4 funds.
- An initial trade was proposed by the State, but NCTCOG staff is proposing the following:
  - Category 2 funding would be removed from four projects (SH 199 in Tarrant County [\$70M], SH 360 at Randol Mill in Tarrant County [\$70M], and US 380 [\$150M] and Spur 399 [\$100M] in Collin County) and placed on the IH 30 Canyon project.
  - Category 12 funds would replace the Category 2 removed from those projects.

# WEST/EAST DISTRIBUTION – CATEGORY 2 AND NON-FORMULA FUNDS

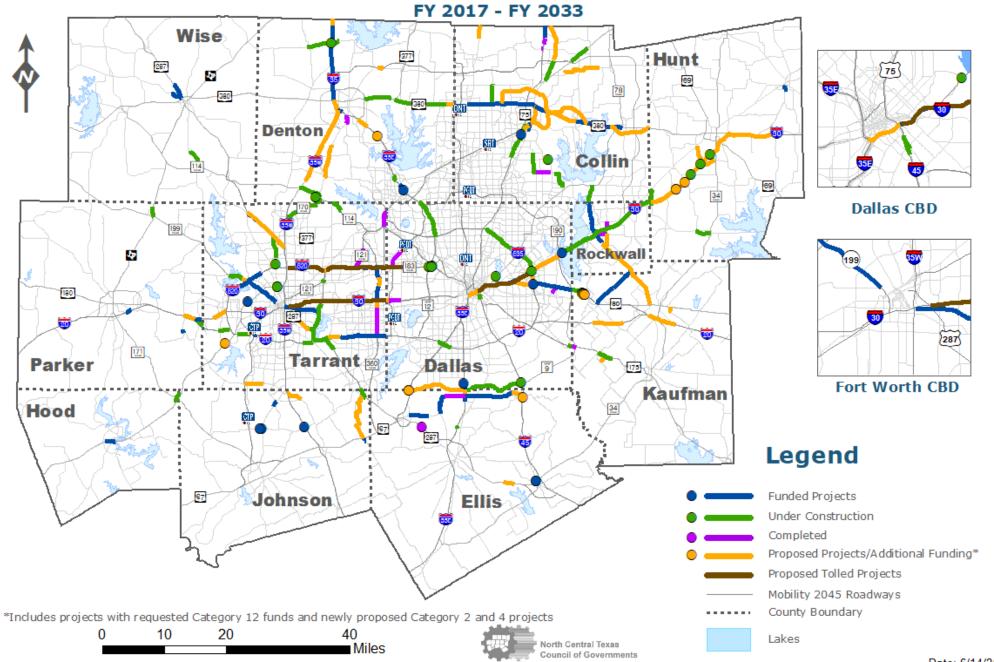
- In coordination with TxDOT, the TIP Team has tracked lettings, change orders, and other charges for Category 2 funding since 2014 to determine carryover for each subregion.
- Coordination with TxDOT is also ongoing regarding the region's non-formula (e.g., Category 12) tracking efforts.
- More details on the region's latest Category 2 carryover balance and non-formula tracking will be provided once coordination with TxDOT and funding decisions have been finalized.

### Dallas-Fort Worth Regional 10 Year Plan Projects

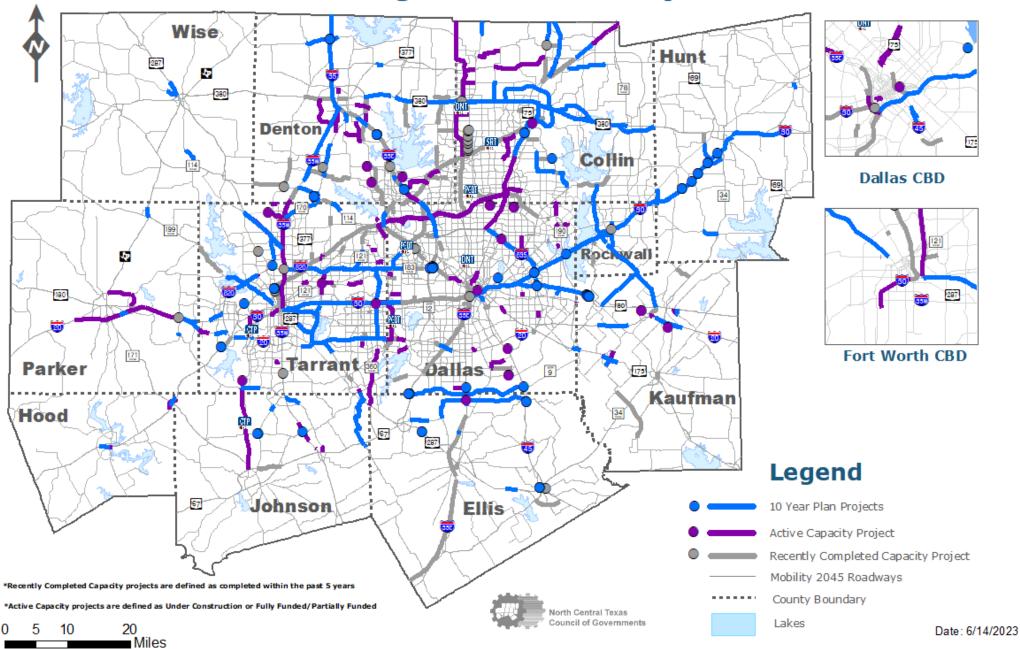
FY 2017 - FY 2033



#### Dallas-Fort Worth Regional 10 Year Plan Project Status



# Recently Completed & Active Capacity Transportation Projects including 10 Year Plan Projects



## **NEXT STEPS**

- Finalize funding amounts in coordination with the TxDOT Districts and TxDOT Headquarters
- Bring back the listings for approval, including changes to the Let/Completed projects list (e.g., 2023 cost overruns)
- Provide an update on the region's Category 2 carryover balances and latest information on West/East non-formula fund tracking
- Bring back any project changes to the committees if the TTC action is different than proposed project listings

## **TIMELINE**

MEETING/TASK	DATE
Funding Targets Received	February 3, 2023
Initial draft list due to TxDOT	February 21, 2023
Feedback Received from TxDOT	May 2023
STTC Information	June 23, 2023
RTC Information	July 13, 2023
Public Involvement	July 2023
STTC Action	July 28, 2023
RTC Action	August 10, 2023
TxDOT Public Involvement for 2024 UTP	July 2023 and August 2023
Anticipated TTC Approval of 2024 UTP	August 2023

### **CONTACT/QUESTIONS?**

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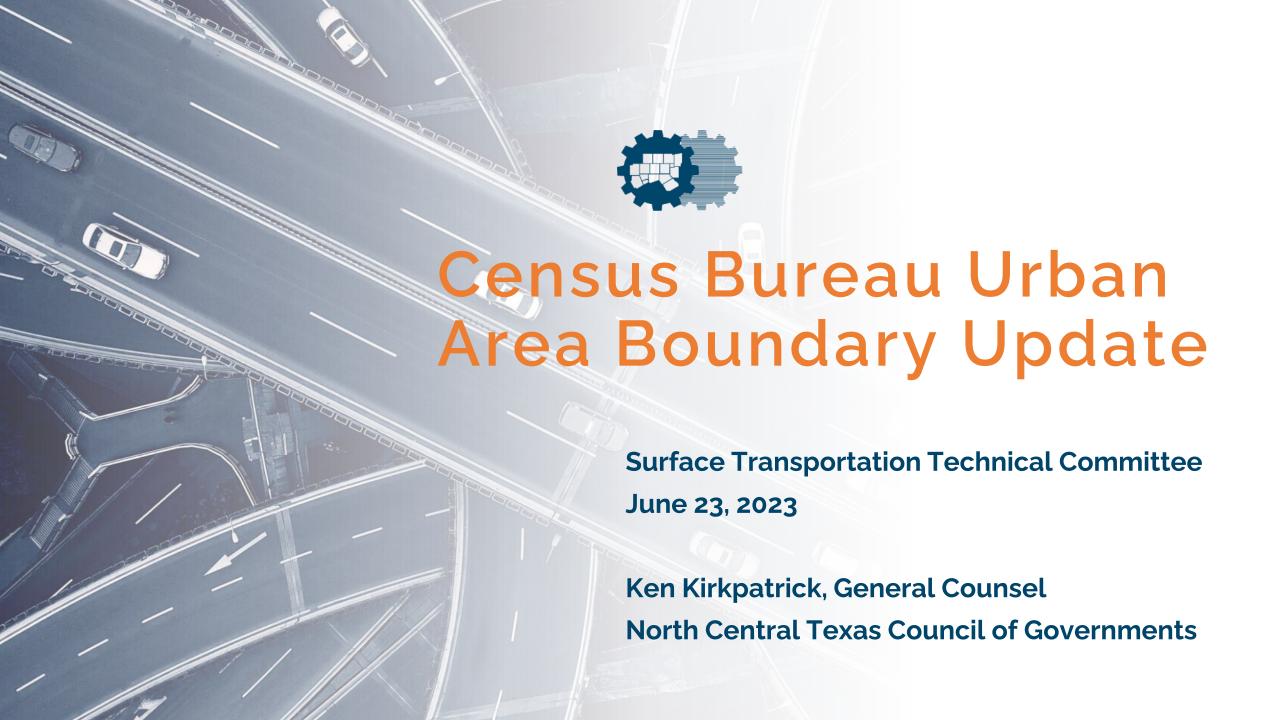
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Cody Derrick Senior Transportation Planner Ph: (817) 608-2391

cderrick@nctcog.org



## **Federal Actions**

#### **US Census Bureau Updates to Urban Areas**

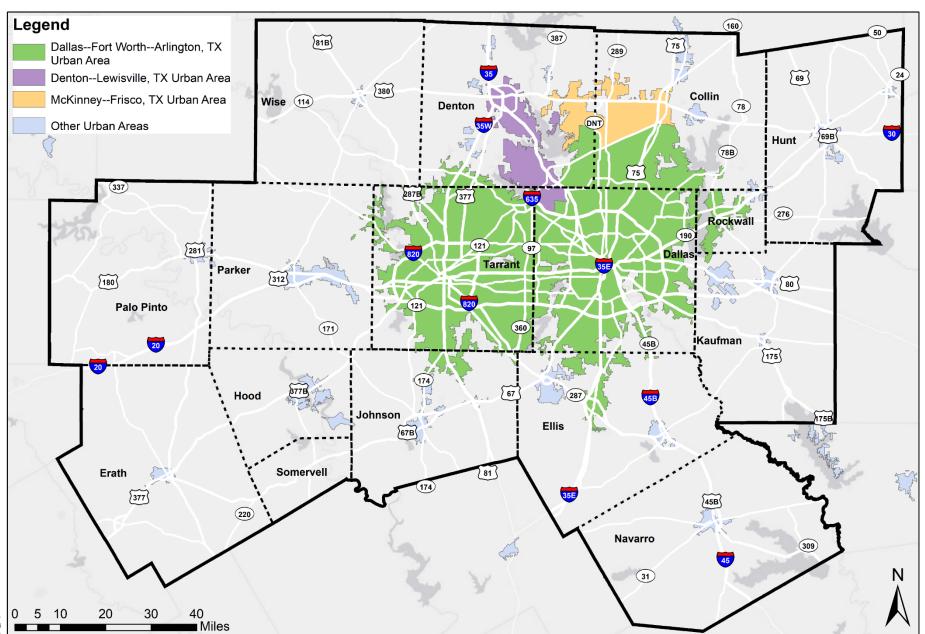
- December 29, 2022
- Based upon 2020 Census of Population and Housing
- Expanded and Renamed McKinney-Frisco Urban Area

#### FHWA/FTA

- June 5, 2023
- Updated Transportation Management Areas
  - McKinney-Frisco (New)
  - Dallas-Fort Worth-Arlington
  - Denton-Lewisville

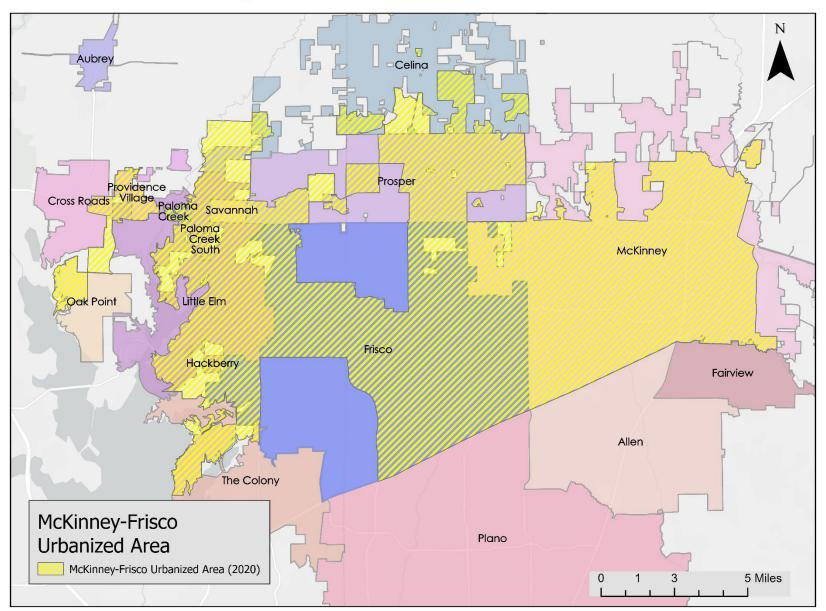


#### U.S. Census Bureau Urban Areas (2020)





# McKinney-Frisco Urbanized Area





# Designated Recipient for FTA Formula Funds

McKinny-Frisco Urbanized Area Needs to Select

Designated Recipient by September 30, 2023

- For apportionments beginning FY 2024
- Selected by Governor, or designee (TxDOT)
  - In Cooperation with transportation providers and the MPO
  - Need Frisco and McKinney Support

#### **Duties of Designated Recipient**

- Principal authority to administer and provide oversight of FTA funds
- Making FTA funds available to eligible projects and entities

**Staff Recommends NCTCOG Serve as Designated Recipient** 



# Designated/Direct recipients by Urbanized Area (as of 2022)

Dallas-Fort Worth-Arlington



# **Anticipated Schedule**

STTC Information June 23, 2023

Frisco City Council July 3, 2023

RTC Information July 13, 2023

McKinney City Council July 18, 2023

STTC Action July 28, 2023

RTC Action August 10, 2023

NCTCOG Executive Board Action August 24, 2023



## **CONTACTS**



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